

FED. ROAD DIV. NO.	STATE	FEDERAL AID PROJECT NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
	RI			1	33

INDEX

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STATE OF RHODE ISLAND

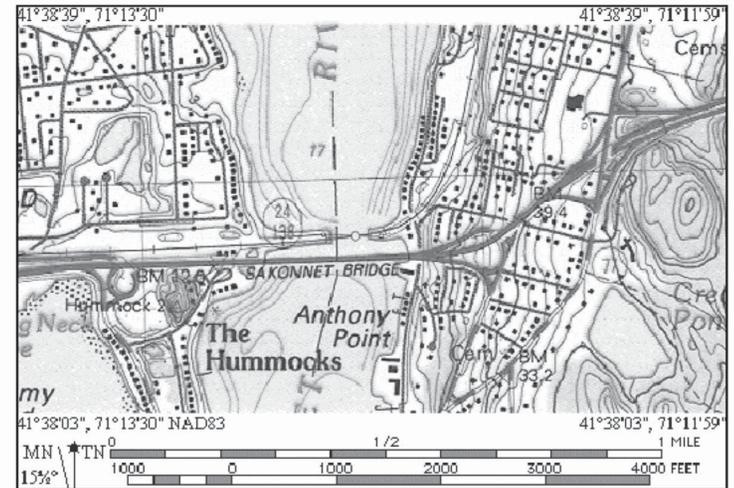


DEPARTMENT OF TRANSPORTATION

PLAN, PROFILE AND SECTIONS OF PROPOSED BRIDGE DEMOLITION

OLD SAKONNET RIVER BRIDGE No. 250

CONTRACT 2 - SUPERSTRUCTURE AND STEEL BENT DEMOLITION

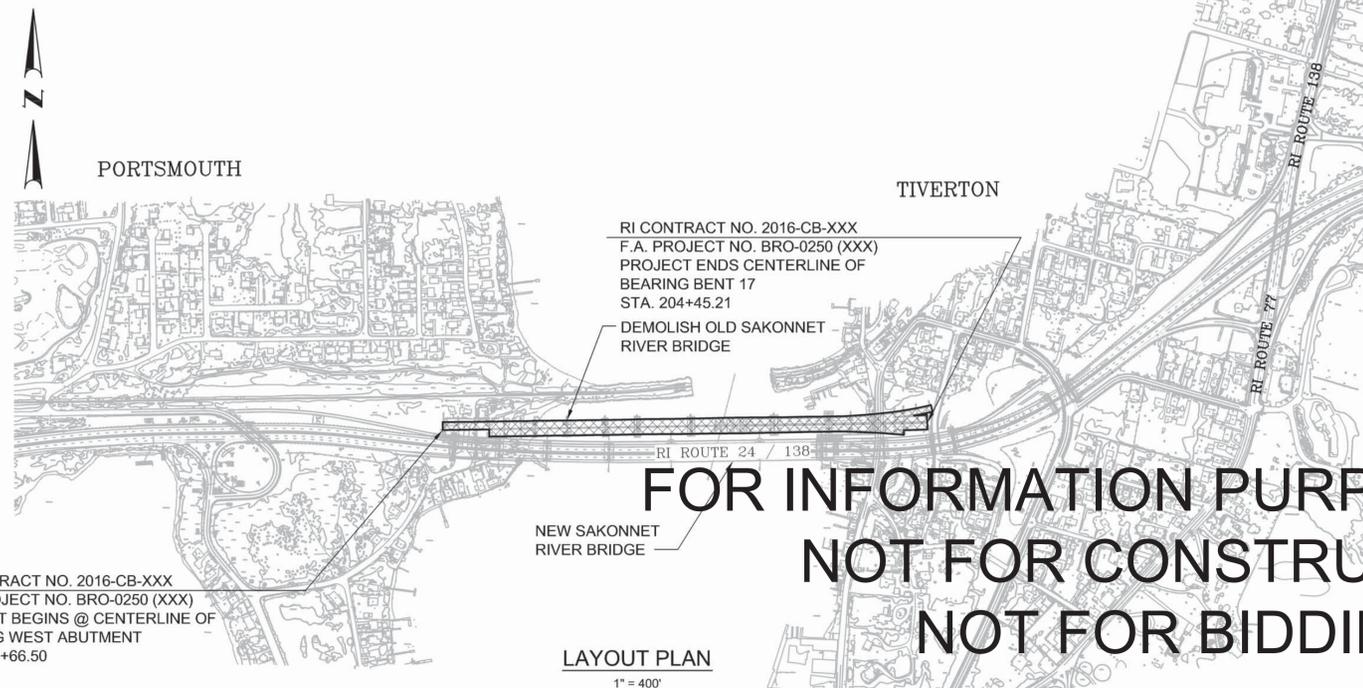


LOCATION MAP
NOT TO SCALE

PORTSMOUTH AND TIVERTON, RHODE ISLAND
COUNTY OF NEWPORT

R.I. CONTRACT NO. 2016-CB-XXX F.A. PROJECT NO. BRO-0250 (XXX)

LENGTH = 0.42 MILES



**FOR INFORMATION PURPOSES ONLY
NOT FOR CONSTRUCTION
NOT FOR BIDDING**

HURRICANE EVACUATION ROUTE
This project includes work on a designated Hurricane Evacuation and Diversionary Route as follows:
- Sakonnet River Bridge (RI Route 24 / 138) within the project limits
Refer to General Notes Note 18.

**CRMC SUBMISSION
AUGUST 2016**

R.I. DEPARTMENT OF TRANSPORTATION

APPROVED _____ DATE _____

DEPUTY CHIEF ENGINEER _____ DATE _____

APPROVED _____

CHIEF ENGINEER _____ DATE _____

APPROVED _____

DIRECTOR _____ DATE _____

DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION

APPROVED _____

DIVISION ADMINISTRATOR _____ DATE _____

R.I. STANDARD SPECIFICATIONS AND STANDARD DETAILS
SPECIFICATIONS TO GOVERN THIS PROJECT ARE THE R.I. STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, AMENDED AUGUST 2013, WITH ALL REVISIONS AND THE STATE AND FEDERAL SPECIAL PROVISIONS INCLUDED IN THE CONTRACT DOCUMENTS. STANDARD DETAILS FOR THIS PROJECT ARE R.I. STANDARD DETAILS, 1998 EDITION, WITH ALL REVISIONS.

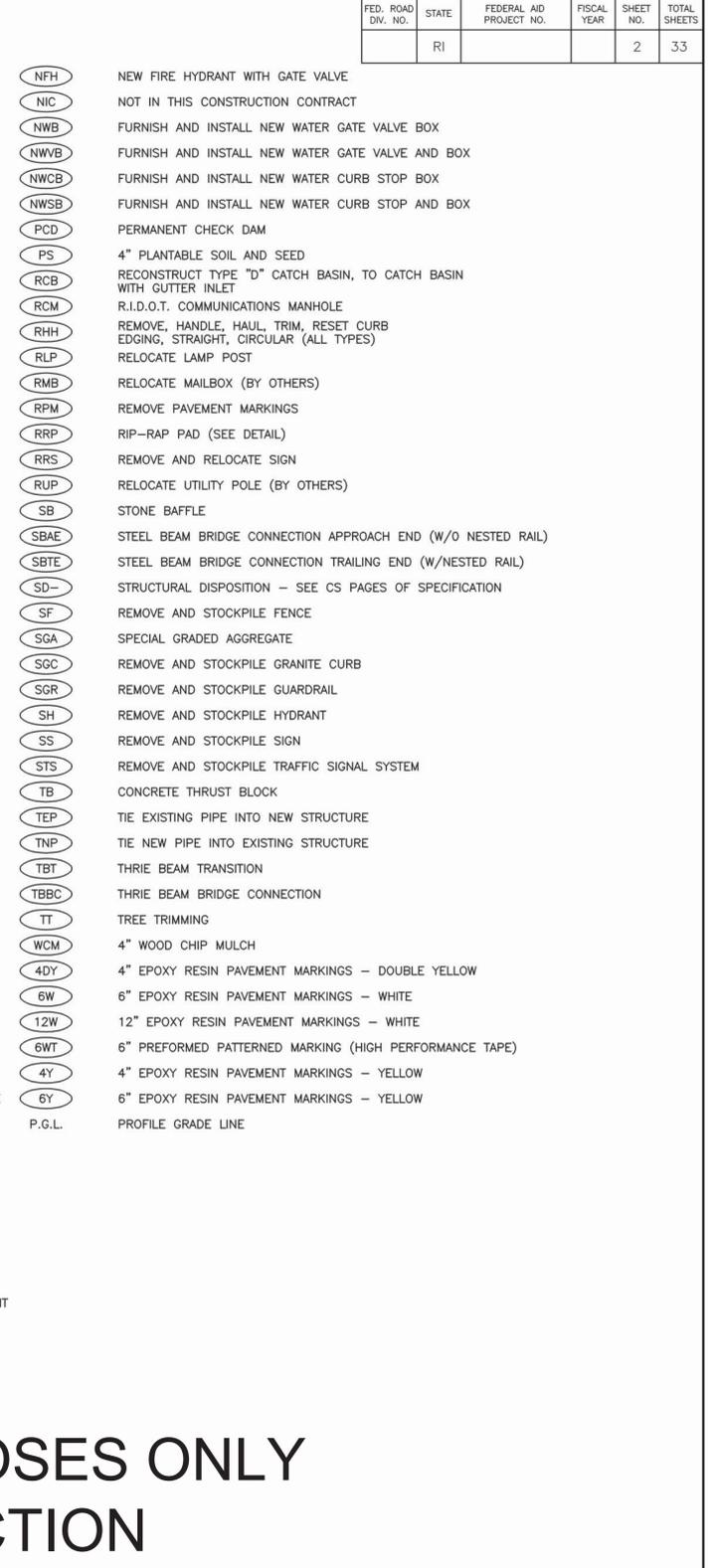
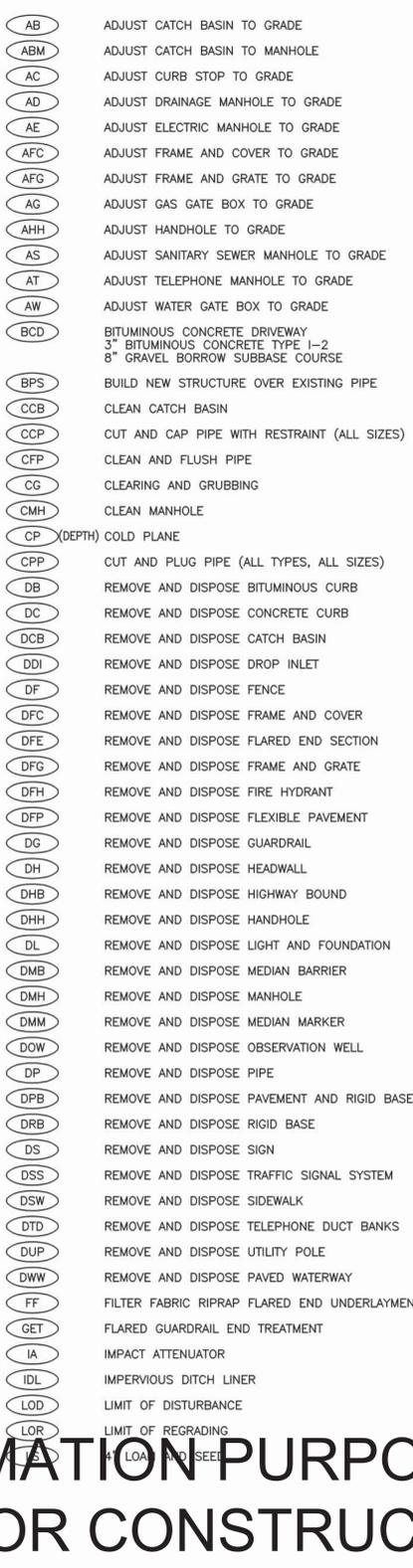
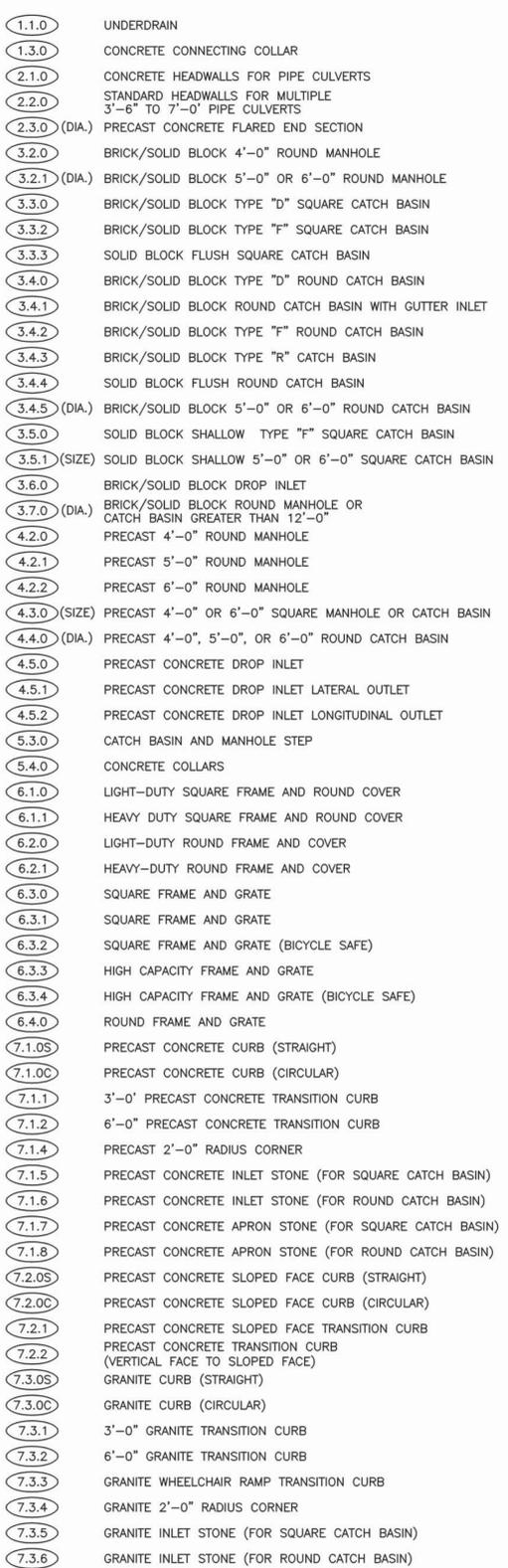
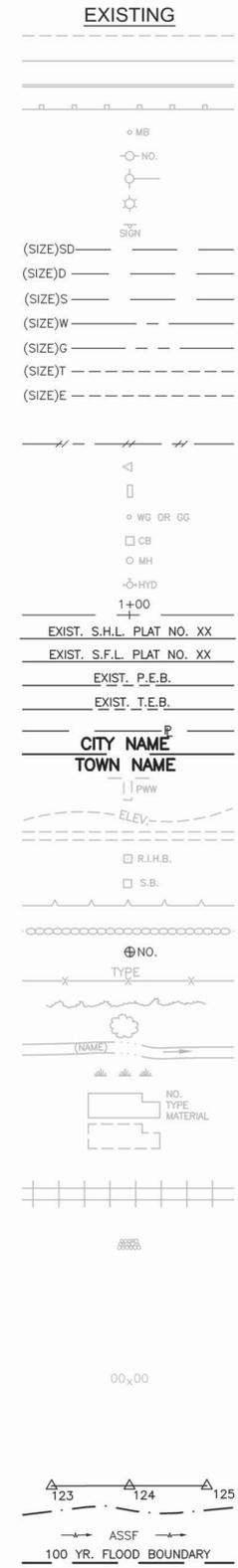
RI CONTRACT NO. 2016-CB-XXX
F.A. PROJECT NO. BRO-0250 (XXX)
PROJECT BEGINS @ CENTERLINE OF BEARING WEST ABUTMENT
STA. 182+66.50

LAYOUT PLAN
1" = 400'

BASE OF LEVELS
NGVD 1929 - VERTICAL
NAD 1983 - HORIZONTAL



Contract Number 2016-CB-XXX
Number of Sheet 1
Total Sheets 33



FOR INFORMATION PURPOSES ONLY
NOT FOR CONSTRUCTION
NOT FOR BIDDING

CEC# 03062.00

COMMONWEALTH ENGINEERS & CONSULTANTS, INC.
400 SMITH STREET
PROVIDENCE, RI 02908



**RHODE ISLAND
DEPARTMENT OF TRANSPORTATION**

REVISIONS			REVISIONS		
NO.	DATE	BY	NO.	DATE	BY
DESIGNED BY:			DATE:		
CHECKED BY:			DATE:		
SCALE:	NONE				

**BRIDGE DEMOLITION
OLD SAKONNET RIVER BRIDGE NO. 250**

PORTSMOUTH / TIVERTON RHODE ISLAND

STANDARD PLAN SYMBOLS & STANDARD LEGEND

SHEET DESIGNATION
G-1

GENERAL NOTES:

1. ANY DAMAGE TO EXISTING PAVEMENT, BRIDGES, CONDUIT, SIDEWALK, FENCES, ETC., CAUSED BY THE CONTRACTOR SHALL BE REPAIRED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE STATE.
2. THE CONTRACTOR SHALL PLACE ALL EQUIPMENT AND MATERIAL AS FAR AWAY AS POSSIBLE FROM THE EDGE OF THE TRAVEL LANE SO AS NOT TO CAUSE A SAFETY HAZARD, IN ACCORDANCE WITH SECTION 106.06 OF THE R.I.D.O.T. STANDARD SPECIFICATION, LATEST EDITION.
3. IT IS THE CONTRACTOR'S RESPONSIBILITY TO ENSURE THAT THE EXISTING CONDITIONS ARE NOT OBLITERATED BEFORE CONTROL POINTS ARE LOCATED AND CONSTRUCTION LAYOUT IS ESTABLISHED. THE CONSTRUCTION LAYOUT SHALL BE PROVIDED IN SUFFICIENT DETAIL, THEREBY ENABLING HIM TO CONSTRUCT THE PROJECT IN CONFORMITY WITH THE PLANS AND SPECIFICATIONS. SURVEY WILL BE PROVIDED BY THE CONTRACTOR. THE RESIDENT ENGINEER WILL NOT AUTHORIZE CONSTRUCTION ACTIVITIES TO BEGIN UNTIL HE IS SATISFIED THAT ALL GROUND CONTROL HAS BEEN ESTABLISHED, TIED DOWN, AND DULY RECORDED IN STANDARD FIELD BOOKS.
4. ALL R.I. STD. 9.9.0 CONSTRUCTION ACCESS ROADS SHALL BE CONSTRUCTED PRIOR TO ANY ROADWAY ACCEPTING CONSTRUCTION TRAFFIC.
5. THE FREQUENCY AND APPLICATION RATES FOR THE DUST CONTROL ITEMS WILL BE AS DIRECTED BY THE ENGINEER.
6. ALL SIDEWALK AND DRIVEWAYS DESIGNATED FOR REPLACEMENT SHALL BE CUT AND MATCHED AT LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
7. ASPHALT EMULSION TACK COAT SHALL BE PLACED PRIOR TO PAVEMENT PLACEMENT ON THE CONCRETE BASE OR COLD PLANED PAVEMENT, AND ON ANY NEW COURSE WHICH HAS BEEN OPEN TO TRAFFIC, OR ANY NEW COURSE WHICH HAS BEEN EXPOSED FOR MORE THAN 3 DAYS, AND/OR AS DIRECTED BY THE ENGINEER. IT SHALL ALSO BE APPLIED TO VERTICAL PAVEMENT FACES BETWEEN ADJOINING PAVEMENT SECTIONS. ALL APPLICATIONS ON BOTH HORIZONTAL AND VERTICAL SURFACES SHALL BE PAID FOR UNDER THE CONTRACT UNIT BID PRICE FOR CODE 403.0300 "ASPHALT EMULSION TACK COAT."
8. THE LIMITS OF CLEARING AND SURFACE DISTURBANCE MUST BE STRICTLY ADHERED TO IN ALL AREAS. IN ADDITION TO THOSE AREAS SPECIFICALLY DESIGNATED ON THE PLANS, THE CONTRACTOR WILL BE RESPONSIBLE FOR PROVIDING AND PLACING, AT HIS OWN EXPENSE, PLANTABLE SOIL AND SEED IN AREAS WHICH ARE OUTSIDE OF THE PROJECT'S AREAS OF DISTURBANCE AND WHICH ARE IMPACTED BY CONSTRUCTION OPERATIONS INCLUDING THOSE AREAS WHERE VEHICLES, EQUIPMENT AND MATERIALS ARE STORED WITH THE PERMISSION OF THE ENGINEER.
9. UNDER NO CIRCUMSTANCE WILL THE CONTRACTOR BE ALLOWED TO STOCKPILE REMOVED PAVEMENT MATERIALS WITHIN THE PROJECT LIMITS.
10. CLEANING AND SWEEPING OF PAVEMENT WILL INCLUDE REMOVAL OF ALL PAVEMENT DEBRIS PRIOR TO THE PLACEMENT OF EACH BITUMINOUS PAVEMENT LIFT. ALL CLEANING AND SWEEPING SHALL BE DONE TO THE SATISFACTION OF THE ENGINEER.
11. PRIOR TO INSTALLATION, ALL SIGNS, MOUNTINGS AND LOCATIONS SHALL BE APPROVED OR MODIFIED BY THE ENGINEER.
12. THE COORDINATE SYSTEM, IF SHOWN, IS THE RHODE ISLAND STATE PLANE COORDINATE SYSTEM.
13. PAVEMENT OPERATIONS FOR CURBED SECTIONS: IN AREAS WHERE CURBING IS SET TO FINISH LINE AND GRADE, THE CONTRACTOR WILL NOT BE REQUIRED TO UTILIZE THE SENSOR AND SKY-TYPE DEVICE FOR AUTOMATIC GRADE CONTROL, BUT WILL BE ALLOWED TO MANUALLY ADJUST THE BITUMINOUS PAVER FOR CONTROLLING GRADE.
14. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING ALL ROADWAYS FREE OF DEBRIS RESULTING FROM THEIR CONSTRUCTION OPERATIONS. ALL DEBRIS SHALL BE REMOVED TO THE SATISFACTION OF THE ENGINEER AT NO ADDITIONAL COST TO THE STATE.
15. NO FUEL STORAGE, VEHICLE REFUELING, OR EQUIPMENT STORAGE SHALL TAKE PLACE IN DESIGNATED WETLANDS, NOR WITHIN 100' OF ANY WATER BODY. THIS REQUIREMENT SHALL NOT SUPERSEDE ANY FEDERAL, STATE OR LOCAL LAW, ORDINANCE, RULE OR REGULATION THAT APPLIES TO THE SAME, UNLESS THIS REQUIREMENT IS MORE STRINGENT THAN SAID LAW, ORDINANCE, RULE OR REGULATION.
16. THE CONTRACTOR SHALL BE RESPONSIBLE TO ENSURE THAT AT THE END OF FINAL PAVING OPERATIONS, FLOW TO EXISTING DRAINAGE STRUCTURES HAS BEEN REESTABLISHED AND THAT NO ISOLATED DEPRESSIONS REMAIN. THERE SHALL BE NO SEPARATE PAYMENT FOR THIS PROVISION; IT SHALL BE CONSIDERED INCIDENTAL TO PAVING AND COLD PLANING OPERATIONS.
17. ALL EMBANKMENTS SHALL BE PLACED IN HORIZONTAL LAYERS NOT EXCEEDING 12" (AFTER COMPACTION) AND SHALL BE COMPACTED AS SPECIFIED BEFORE THE NEXT LAYER IS PLACED. ALSO, EMBANKMENT CONSTRUCTION SHALL CONFORM TO SECTION 202.03.2 OF THE R.I.D.O.T. STANDARD SPECIFICATIONS, LATEST EDITION.
18. IF THIS PROJECT IS ON A HURRICANE EVACUATION AND DIVERSIONARY ROUTE, AS DESIGNATED ON THE COVERSHEET, THE CONTRACTOR IS ADVISED THAT UPON 12 (TWELVE) HOURS NOTICE THE ROADWAY SHALL BE OPEN TO EVACUEES AND EMERGENCY PERSONNEL. ANY EXTRA WORK NECESSARY TO COMPLY WITH THIS REQUIREMENT WILL BE REIMBURSED UNDER FORCE ACCOUNT PROCEDURES.
19. THE CONTRACTOR SHALL READ, BECOME FAMILIAR WITH, AND ADHERE TO ALL OF THE PROVISIONS, CONDITIONS, AND STIPULATIONS STATED IN THE ENVIRONMENTAL APPROVALS ISSUED FOR THE PROJECT FROM THE DEPARTMENT OF ENVIRONMENTAL MANAGEMENT (RIDEM), AND/OR THE ARMY CORPS OF ENGINEERS (ACOE), AND/OR THE COASTAL RESOURCES MANAGEMENT COUNCIL (CRMC), COPIES OF EACH OF THESE PERMITS ARE INCLUDED IN THE CS PAGES OF THE CONTRACT DOCUMENTS. ALL COSTS ASSOCIATED WITH THESE CONDITIONS SHALL BE CONSIDERED INCIDENTAL TO THE CONSTRUCTION AND INCLUDED WITH THE COST FOR THE ASSOCIATED BID ITEM(S).
20. FOR ALL PROJECTS INVOLVING KNOWN SITE REMEDIATION ISSUES, THE CONTRACTOR SHALL READ, BECOME FAMILIAR WITH, AND ADHERE TO ALL OF THE CONSTRUCTION RELATED PROVISIONS, CONDITIONS, AND STIPULATIONS OF ANY REMEDIAL PLANS DEVELOPED FOR THE PROJECT. COPIES OF THESE DOCUMENTS ARE INCLUDED IN THE CS PAGES OF THE CONTRACT DOCUMENTS. ALL COSTS ASSOCIATED WITH COMPLIANCE WITH THESE DOCUMENTS SHALL BE CONSIDERED INCIDENTAL TO THE CONSTRUCTION AND INCLUDED WITH THE COST FOR THE ASSOCIATED BID ITEM(S).
21. NO UNPROTECTED CONSTRUCTED FEATURE MAY PROJECT MORE THAN 4 INCHES ABOVE THE FINISHED GRADE OF A TRAVERSABLE SLOPE IN A CLEAR ZONE, e.g. HEADWALL, DRAINAGE INLET, ETC.
22. THE REMAINING SECTION OR STUB OF A BREAKAWAY BASE MAY NOT PROJECT MORE THAN 4 INCHES ABOVE THE FINISHED GRADE OF A TRAVERSABLE SLOPE IN A CLEAR ZONE, e.g. SIGN POSTS, LIGHT POLES, FIRE HYDRANTS, ETC.

DRAINAGE AND EROSION CONTROL NOTES:

1. FOR ALL PROJECTS WITH AT LEAST ONE(1) ACRE OF SOIL DISTURBANCE, R.I.D.O.T. IS REQUIRED TO DEVELOP AND ENFORCE A SITE SPECIFIC STORM WATER POLLUTION PREVENTION PLAN (SWPPP) IN ORDER TO REMAIN IN COMPLIANCE WITH THE RIDES GENERAL PERMIT FOR STORMWATER DISCHARGES ASSOCIATED WITH CONSTRUCTION ACTIVITIES. THE CONTRACTOR SHALL READ, BECOME FAMILIAR WITH, AND ADHERE TO ALL OF THE PROVISIONS, CONDITIONS, AND STIPULATIONS OF THE GENERAL PERMIT AND THE SITE SPECIFIC SWPPP FOR THIS PROJECT. COPIES OF THESE DOCUMENTS ARE INCLUDED IN THE CS PAGES OF THE CONTRACT DOCUMENTS. ALL COSTS ASSOCIATED WITH ADHERENCE TO THE SWPPP SHALL BE CONSIDERED INCIDENTAL TO THE CONSTRUCTION AND INCLUDED WITH THE COST FOR THE ASSOCIATED BID ITEM(S).
2. NO UNDISTURBED AREAS SHALL BE CLEARED OF EXISTING VEGETATION AFTER OCTOBER 15 OF ANY CALENDAR YEAR OR DURING ANY PERIOD OF FULL OR LIMITED WINTER SHUTDOWN. ALL DISTURBED SOILS EXPOSED PRIOR TO OCTOBER 15 OF ANY CALENDAR YEAR SHALL BE SEEDED OR PROTECTED BY THAT DATE. ANY SUCH AREAS THAT DO NOT HAVE ADEQUATE VEGETATIVE STABILIZATION, AS DETERMINED BY THE RESIDENT ENGINEER OR ENVIRONMENTAL INSPECTOR, BY NOVEMBER 15 OF ANY CALENDAR YEAR, MUST BE STABILIZED THROUGH THE USE OF EROSION CONTROL MATTING OR HAY MULCH, IN ACCORDANCE WITH SPECIFICATIONS CONTAINED WITHIN THE R.I. SOIL EROSION AND SEDIMENT CONTROL HANDBOOK. IF WORK CONTINUES WITHIN ANY OF THESE AREAS DURING THE PERIOD FROM OCTOBER 15 THROUGH APRIL 15, CARE MUST BE TAKEN TO ENSURE THAT ONLY THE AREA REQUIRED FOR THAT DAY'S WORK IS EXPOSED, AND ALL ERODIBLE SOIL MUST BE REESTABLISHED WITHIN 5 WORKING DAYS. ANY WORK TO CORRECT PROBLEMS RESULTING FROM FAILURE TO COMPLY WITH THIS PROVISION SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. THERE WILL BE NO SEPARATE PAYMENT FOR THIS PROVISION, IT SHALL BE CONSIDERED INCIDENTAL TO CONSTRUCTION OPERATIONS. STABILIZATION OF ONE FORM OR ANOTHER AS DESCRIBED ABOVE SHALL BE ACHIEVED WITHIN 2 WEEKS OF FINAL GRADING.
3. STOCKPILES OF MATERIAL SHALL NOT BE LOCATED WITHIN REGULATED WETLANDS OR BUFFER ZONE AREAS. THEY SHALL HAVE SIDE SLOPES NO GREATER THAN 30% AND STOCKPILES OF ERODABLE MATERIAL SHALL ALSO BE SEEDED AND RINGED WITH R.I. STD. 9.1.0 TO STABILIZE.
4. IF THE PLANS INCLUDE SPECIFIC AREAS FOR PLACEMENT OF CONSTRUCTION DEWATERING BASINS AND/OR EQUIPMENT AND MATERIALS STORAGE AND STOCKPILING, AND IF THE CONTRACTOR ELECTS TO UTILIZE ANY OTHER AREAS FOR THESE PURPOSES, THIS SHALL BE APPROVED BY THE ENGINEER ONLY AFTER OBTAINING ANY NECESSARY PERMITS AND/OR PERMIT MODIFICATIONS FROM THE APPROPRIATE REGULATORY AUTHORITY(IES). ANY PERMITTING REQUIREMENTS SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR AND SHALL BE ACCOMPLISHED AT NO COST TO THE STATE. THE ENGINEER WILL COORDINATE SUBMISSION OF ANY REQUIRED PERMIT APPLICATION MATERIALS WITH THE R.I.D.O.T. OFFICE OF ENVIRONMENTAL PROGRAMS.
5. JUTE MESH SHALL BE USED TO STABILIZE PLANTABLE SOIL AND/OR LOAM IN ALL DITCHES, ON ALL SLOPES ADJACENT TO WETLANDS AND WETLAND PERIMETERS, AND ON ALL SLOPES WITHIN WATER QUALITY BASINS. JUTE MESH IN DITCHES SHALL EXTEND TO AN ELEVATION 2 FEET ABOVE THE BOTTOM OF THE DITCH.
6. SEEDING ON ALL SLOPES 3 TO 1 OR STEEPER SHALL CONSIST OF THE FOLLOWING APPLICATIONS UNLESS CHANGED IN THE CONTRACT.
 - a. SEEDING TYPE I.
 - b. ADHESIVE MULCH STABILIZER
7. UNVEGETATED SLOPES SHALL NOT BE UNATTENDED OR EXPOSED FOR PERIODS IN EXCESS OF 2 WEEKS OR THROUGH THE INACTIVE WINTER SEASON.
8. PRIOR TO DRAINAGE AND UTILITY CONSTRUCTION, THE CONTRACTOR IS RESPONSIBLE FOR VERIFYING THE LOCATION (HORIZONTAL AND VERTICAL) OF ALL EXISTING PIPES AND/OR STRUCTURES WHICH ARE TO BE CONNECTED. ANY VARIATION FOUND FROM THE PLANS MUST BE BROUGHT TO THE ENGINEER'S ATTENTION PRIOR TO DRAINAGE AND UTILITY CONSTRUCTION. WORK CAN COMMENCE ONLY UPON THE ENGINEER'S AUTHORIZATION.
9. ALL DRAINAGE AND UTILITY STRUCTURES WITHIN THE PAVED ROADWAY SHALL BE ADJUSTED TO GRADE WITH THE SURROUNDING PAVEMENT PRIOR TO THE WINTER SHUTDOWN.
10. DURING CONSTRUCTION, THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING DRAINAGE AND RUNOFF FLOW DURING STORMS AND PERIODS OF RAINFALL THROUGHOUT THE WORK AREA.
11. CATCH BASIN RIM GRADES NOTED ON PLANS ARE DEPRESSED 0.1' LOWER THAN THE GUTTER GRADE. RIM ELEVATIONS SHOWN ARE FINAL GRADES. THE CONTRACTOR SHALL PLACE FRAMES AND GRATES 0.1' BELOW THE GRADE CONSTRUCTED IN THIS CONTRACT OR AS DIRECTED BY THE ENGINEER.
12. PROVISIONS FOR CLEARING TO ACCESS OUTFALLS DURING THE CLEANING AND FLUSHING OF THE CLOSED DRAINAGE SYSTEM SHALL BE KEPT TO A MINIMUM.
 - a. ANY VEGETATIVE CLEARING SHALL BE LIMITED TO BRUSH AND TREES LESS THAN 3" DIAMETER.
 - b. NO HEAVY EQUIPMENT MAY ENCRDACH UPON VEGETATED PERIMETER OR RIVERBANK WETLANDS AS WELL AS BIOLOGICAL WETLANDS.
13. THE CONTRACTOR SHALL INSTALL ALL EROSION CONTROL DEVICES FOR OUTLET PROTECTION PRIOR TO CLEANING AND FLUSHING STORM WATER DRAINAGE. EROSION CONTROL DEVICES SHALL REMAIN IN PLACE UNTIL ALL FLUSHED SEDIMENTS ARE REMOVED. AT ALL OUTFALL LOCATIONS WHERE PIPES ARE TO BE CLEANED AND FLUSHED, OUTLET PROTECTION (R.I. STD. 9.1.0 OR 9.3.0) SHALL BE INSTALLED TO TRAP SEDIMENTS. THESE SEDIMENTS SHALL THEN BE REMOVED AND DISPOSED OF LEGALLY BEFORE THE OUTLET PROTECTION DEVICES ARE REMOVED. IF OUTLET PROTECTION AT THE OUTFALL IS NOT FEASIBLE, THEN THE OUTLET PIPE OF THE LAST DRAINAGE STRUCTURE TO BE CLEANED SHALL BE PLUGGED TO CAPTURE ALL MATERIALS FLUSHED FROM PIPES. AFTER THE MATERIALS ARE REMOVED FROM THE DRAINAGE STRUCTURE, THE OUTLET SHALL BE UNPLUGGED TO RESUME NORMAL FUNCTIONING.
14. R.I. STD. 9.8.0 BALED HAY INLET PROTECTION SHALL BE INSTALLED AT ALL CATCH BASINS AND INLETS WHENEVER SUBBASE IS EXPOSED, AND SHALL REMAIN IN PLACE UNTIL THE ABUTTING GROUND SURFACES ARE STABILIZED.
15. WHERE BALED HAY INLET PROTECTION AND SILT FENCES ARE USED AT CATCH BASINS, THEY SHALL BE REMOVED AT THE END OF THE PROJECT OR AS DIRECTED BY THE ENGINEER IN ORDER TO PREVENT CLOGGING OF THE INLET.

DRAINAGE AND EROSION CONTROL NOTES (CONTINUED):

16. DETENTION AND RETENTION BASINS MAY BE ROUGH GRADED AND STABILIZED WITH VEGETATION AND/OR OTHER EROSION CONTROL MEASURES AS REQUIRED BY THE ENGINEER PRIOR TO USE AS TEMPORARY SEDIMENTATION BASINS DURING PROJECT CONSTRUCTION. FINAL BASIN CONSTRUCTION SHALL NOT COMMENCE UNTIL ALL SOURCES OF SEDIMENT HAVE BEEN ELIMINATED, FINAL ROADSIDE VEGETATION IS ESTABLISHED AND USE OF TEMPORARY BASINS IS NO LONGER REQUIRED AS DIRECTED BY THE ENGINEER. ANY ISSUES RELATING TO EROSION AND/OR SEDIMENT TRANSPORT INTO WETLAND AREAS RESULTING FROM SUCH USE OF SEDIMENTATION BASINS DURING CONSTRUCTION SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. ANY CORRECTIVE ACTION REQUIRED TO RESOLVE SUCH ISSUES SHALL BE COMPLETED BY THE CONTRACTOR.
17. THE TOE OF ANY FILL SLOPE IS TO REMAIN AT LEAST 1' INSIDE OF ALL EROSION CONTROLS. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR COVER ANY PORTION OF THE EROSION CONTROL MEASURES WITH MATERIAL. ANY MATERIAL THAT IS PLACED ON ANY EROSION CONTROLS BY THE CONTRACTOR, OR ANY AGENT OF THE CONTRACTOR, SHALL BE IMMEDIATELY REMOVED BY THE CONTRACTOR, AND ANY NECESSARY REPAIRS TO THE EROSION CONTROLS ACCOMPLISHED.
18. PRIOR TO COMMENCING CONSTRUCTION ACTIVITIES, EROSION AND SEDIMENTATION CONTROLS SHALL BE INSTALLED AT THOSE AREAS INDICATED ON THE PLANS. CLEARING MAY OCCUR PRIOR TO INSTALLATION OF SUCH CONTROLS, HOWEVER NO GRUBBING, GRADING, FILLING, OR OTHER SOIL DISTURBANCE SHALL OCCUR PRIOR TO INSTALLATION. THE LIMITS OF CLEARING AND SURFACE DISTURBANCE MUST BE STRICTLY ADHERED TO IN ALL AREAS.
19. ALL HAY BALES, SILT FENCE OR TEMPORARY PROTECTION SHALL REMAIN IN PLACE UNTIL AN ACCEPTABLE STAND OF GRASS IS ESTABLISHED. IF NEEDED, TEMPORARY SEEDING CAN HELP TO MINIMIZE EROSION. TEMPORARY SEED WILL CONFORM TO R.I.D.O.T. STANDARD TEMPORARY SEED MIX.
20. THE CONTRACTOR MUST REPAIR AND/OR RESEED ANY AREAS THAT DO NOT DEVELOP WITHIN THE PERIOD OF ONE YEAR AND HE SHALL DO SO AT NO ADDITIONAL EXPENSE TO THE STATE.
21. THE NORMAL ACCEPTABLE SEASONAL SEEDING DATES ARE SPECIFIED IN SUBSECTION L.02.03 OF THE R.I.D.O.T. STANDARD SPECIFICATIONS, LATEST EDITION.
22. ADDITIONAL EROSION CONTROLS, SHALL BE INSTALLED AS DIRECTED BY THE RESIDENT ENGINEER. THESE ADDITIONAL ITEMS WILL BE PAID AT THE UNIT PRICE FOR THAT BID ITEM.

UTILITY NOTES:

1. EXISTING UTILITIES HAVE BEEN SHOWN ON THE PLANS USING THE BEST AVAILABLE INFORMATION AND ARE APPROXIMATE. BUILDING SERVICE CONNECTIONS (ELECTRIC, GAS, TELEPHONE, WATER AND SANITARY) ARE NOT SHOWN. CONTRACTOR IS TO ASSUME SERVICES ARE PRESENT TO ALL BUILDINGS.
2. THE CONTRACTOR SHALL VERIFY THE LOCATIONS OF ALL EXISTING DRAINAGE AND UTILITIES BOTH UNDERGROUND AND OVERHEAD BEFORE EXCAVATION BEGINS IN ACCORDANCE WITH CHAPTER 39-1.2 OF THE R.I. GENERAL LAWS ENTITLED "EXCAVATION NEAR UNDERGROUND UTILITY FACILITIES", WITH AMENDMENTS EFFECTIVE AS OF NOVEMBER 1, 2009 AND, WHEN NECESSARY, BY CONTACTING THE INDIVIDUAL UTILITY COMPANIES. EXCAVATION SHALL BE IN ACCORDANCE WITH ALL STATUTES, ORDINANCES, RULES AND REGULATIONS OF ANY APPLICABLE CITY, TOWN, STATE OR FEDERAL AGENCY. THE CONTRACTOR SHOULD UNDERSTAND THAT NOT ALL UTILITIES SUBSCRIBE TO THE DIG SAFE PROGRAM. IT IS THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY ALL UTILITY COMPANIES AND ENSURE THAT ALL UTILITIES HAVE BEEN MARKED PRIOR TO COMMENCING THEIR WORK. ANY DAMAGE TO EXISTING UTILITIES MARKED IN THE FIELD, OR AS A RESULT OF FAILING TO CONTACT THE APPROPRIATE UTILITY COMPANY, SHALL BE REPAIRED OR REPLACED AT NO ADDITIONAL COST TO THE STATE.
3. ALL EXISTING UTILITIES TO BE ABANDONED SHALL BE CAPPED.
4. ALL UTILITY POLE RELATED WORK SHALL BE BY OTHERS.

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	RI			3	33

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CEC# 03062.00



COMMONWEALTH
ENGINEERS & CONSULTANTS, INC.
400 SMITH STREET
PROVIDENCE, RI 02908



**RHODE ISLAND
DEPARTMENT OF TRANSPORTATION**

REVISIONS			REVISIONS		
NO.	DATE	BY	NO.	DATE	BY
1	4/07	TRB			
2	3/10	RBH			
3	4/14	MLP			

DESIGNED BY: _____ DATE: _____
 CHECKED BY: _____ DATE: _____
 SCALE: NONE

**BRIDGE DEMOLITION
OLD SAKONNET RIVER BRIDGE NO. 250**

PORTSMOUTH / TIVERTON
RHODE ISLAND

STANDARD NOTES - 1

SHEET DESIGNATION
G-2

LANDSCAPE NOTES:

- ALL PLANT MATERIAL MUST BE TAGGED AT THE NURSERY (A RECOGNIZED GROWER OF PLANT MATERIAL) IN ACCORDANCE WITH THE R.I.D.O.T. STANDARD SPECIFICATIONS, LATEST EDITION. ALL PLANT MATERIAL MUST BE NURSERY GROWN; NO PLANTATION GROWN PLANT MATERIAL WILL BE ACCEPTED.
- ALL PLANT SUBSTITUTIONS AND/OR CHANGES IN PLANT LOCATION MUST BE APPROVED IN ACCORDANCE WITH THE R.I.D.O.T. STANDARD SPECIFICATIONS, LATEST EDITION.
- ALL PLANT MATERIAL IS TO BE FIELD LOCATED BY A REPRESENTATIVE FROM THE R.I.D.O.T. LANDSCAPE ARCHITECTURE UNIT.
- A R.I.D.O.T. LANDSCAPE REPRESENTATIVE MUST BE ON SITE TO APPROVE ALL TRIMMING AND CLEARING NECESSARY TO COMPLETE THE WORK AS SHOWN ON THE PLANS.
- ANY TOPSOIL USED AS PLANTABLE SOIL SHALL HAVE A SANDY LOAM TEXTURE RELATIVELY FREE OF SUBSOIL MATERIAL, STONES, ROOTS, LUMPS OF SOIL, TREE LIMBS, TRASH OR CONSTRUCTION DEBRIS, AND SHALL CONFORM TO SECTION M.18 OF THE R.I.D.O.T. STANDARD SPECIFICATIONS, LATEST EDITION.
- ALL TREES AND SHRUBS SHALL BE MULCHED WITH PINE BARK MULCH IN ACCORDANCE WITH THE R.I.D.O.T. STANDARD SPECIFICATIONS, LATEST EDITION.
- ALL TREES AND/OR SHRUBS THAT ARE PLANTED AS A BED SHALL BE MULCHED AS A BED.
- PROVIDE A MINIMUM 6'-8" BRANCHING STANDARD ON ALL TREES INSTALLED ADJACENT TO SIDEWALKS AND/OR PEDESTRIAN ACCESS AREAS.

STRUCTURAL NOTES FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS:

GENERAL

- ALL SUPPORT DESIGNS AND ASSOCIATED SHOP DRAWING REVIEWS SHALL BE IN CONFORMANCE WITH THE LATEST EDITION OF THE AASHTO STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS (THE "SPECIFICATIONS"), INCLUDING THE LATEST INTERIM SPECIFICATIONS, EXCEPT AS MODIFIED HEREIN.

CONSTRUCTION DRAWINGS AND DETAILS

- THE FOLLOWING NOTES SHALL BE INCLUDED ON ALL PLANS AND/OR SHOP DRAWINGS IN REFERENCE TO ANCHOR BOLTS:
 - "PRETENSIONING OF ALL ANCHOR NUTS IS REQUIRED, AND SHALL BE ACCOMPLISHED BY TIGHTENING TO 1/6TH TURN BEYOND THE SNUG-TIGHT POSITION."
 - "THE MAXIMUM CLEARANCE BETWEEN THE BOTTOM OF THE LEVELING NUTS AND THE TOP OF THE CONCRETE IS CRITICAL AND SHALL NOT EXCEED THE AMOUNT SPECIFIED ON THIS DRAWING."
- THE USE OF GROUT UNDER BASE PLATES SHALL GENERALLY NOT BE PERMITTED. IF SPECIFIC CONDITIONS WARRANT ITS USE, THE GROUT SHALL NOT BE CONSIDERED LOAD CARRYING; LOADS SHALL BE DIRECTLY SUPPORTED BY THE ANCHOR BOLTS. ADEQUATE DRAINAGE SHALL BE PROVIDED.
- THE DAMPENING EFFECTS OF VIBRATION MITIGATION DEVICES SHALL NOT BE CONSIDERED IN THE DESIGN OF STRUCTURAL SUPPORTS FOR SIGNS AND TRAFFIC SIGNALS. IF THE CONTRACTOR CHOOSES TO USE THESE DEVICES FOR WARRANTY PURPOSES, THE TYPE OF DEVICES PROPOSED SHALL BE APPROVED BY THE DEPARTMENT PRIOR TO FABRICATION OF SUPPORTS.

TRAFFIC SIGNAL NOTES:

- ALL SALVAGED TRAFFIC SIGNAL EQUIPMENT SHALL BE DELIVERED TO THE R.I.D.O.T. MAINTENANCE HEADQUARTERS, 360 LINCOLN AVENUE, WARWICK, RHODE ISLAND, 02888.
- BACK PLATES SHALL BE INSTALLED ON ALL TRAFFIC SIGNAL HEADS.
- THE CONTRACTOR SHALL SUPPLY AND INSTALL ON THE UPPER LEFT HAND CORNER OF THE BACK OF THE CONTROLLER CABINET DOOR A LAMINATED INTERSECTION GRAPHIC AND TABLE DEPICTING THE TRAFFIC DETECTOR RELAY CHANNEL ASSIGNMENTS. THE DIAGRAM SHALL BE A GRAPHIC OF THE INDIVIDUAL INTERSECTION ORIENTED SIMILAR TO THE PLANS SHOWING THE LOCATIONS OF EACH OF THE LOOP DETECTORS. THE DIAGRAM SHALL, AT A MINIMUM, INCLUDE DETECTOR NUMBERS, STREET NAME LABELS, NORTH ARROW, AND CONTROLLER CABINET LOCATION. THE ASSIGNMENT INFORMATION SHALL BE INCLUDED IN A TABLE WHICH SHALL INCLUDE, AT A MINIMUM, THE APPROACH NAME, DETECTOR NUMBER, TERMINAL NUMBER, DETECTOR RACK SLOT NUMBER, RELAY NUMBER, RELAY CHANNEL NUMBER, AND PHASE ASSOCIATED WITH EACH DETECTOR.
- TRAFFIC CONTROLLER CABINETS, UNLESS OTHERWISE NOTED, SHALL BE NEMA TS2 TYPE 1 CABINET SIZE 6 ("P" TYPE) WITH NOMINAL DIMENSIONS OF 52"x44"x24"D.
- ALL DELAY AND EXTENSION TIMES, AS CALLED FOR ON THE PLANS, FOR PROPOSED LOOP DETECTORS SHALL BE PROGRAMMED IN THE TRAFFIC SIGNAL CONTROLLER AND NOT THE DETECTOR RELAY.
- A BARE GROUND WIRE SHALL BE PLACED IN ALL PVC CONDUITS AND SHALL BE BONDED TO GROUND RODS IN ACCORDANCE WITH SECTION T.03 OF THE RHODE ISLAND DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.
- THE FINAL POSITION OF SIGNAL HEADS, PEDESTRIAN PUSHBUTTONS, DETECTORS, AND STOP LINE AND CROSSWALK PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER IN THE FIELD ACCORDING TO ACTUAL INTERSECTION CHARACTERISTICS.
- A 2' MINIMUM BUFFER SHALL BE PROVIDED BETWEEN THE CURB AND ALL LATERAL OBSTRUCTIONS (INCLUDING ALL SIGNAL POLES AND TRAFFIC/PEDESTRIAN SIGNAL HEADS) TO PROVIDE ADEQUATE CLEARANCE FOR TURNING VEHICLES.
- ALL FOUNDATIONS MUST HAVE CONES OR BARRELS BOLTED TO FOUNDATION BASES UNTIL ACTUAL POLE IS INSTALLED.
- WHEN PLACING TRAFFIC SIGNAL HANDHOLES OR CONDUIT IN EXISTING PORTLAND CEMENT CONCRETE SIDEWALKS, THE ENTIRE SIDEWALK SQUARE OF CONCRETE SHALL BE REPLACED IN ACCORDANCE WITH R.I. STD. 43.1.0. NO PATCHES WILL BE ALLOWED.
- ALL PEDESTRIAN PUSHBUTTONS SHALL BE COMPLIANT WITH "THE AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES" (ADAAG) AND SHALL INCLUDE A PRESSURE-ACTIVATED (NON-MOVING) BUTTON. SIGNS APPLICABLE TO PUSHBUTTON ACTUATION SHALL BE INSTALLED SUCH THAT THE CROSSING ASSIGNED TO EACH BUTTON IS CLEARLY INDICATED. IF SITE CONDITIONS DO NOT ALLOW PEDESTRIAN PUSHBUTTONS TO BE INSTALLED WHERE CALLED FOR ON THE PLANS, THE R.I.D.O.T. TRAFFIC ENGINEERING UNIT SHALL BE CONSULTED WITH THROUGH AN R.F.I. PRIOR TO INSTALLING THE PUSHBUTTONS. THE FINAL PLACEMENT OF ALL PEDESTRIAN PUSHBUTTONS SHALL BE IN ACCORDANCE WITH ADAAG AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION.
- ALL LOOP DETECTORS SHALL BE CENTERED WITHIN EACH LANE AS DELINEATED, UNLESS OTHERWISE DIMENSIONED ON PLANS.
- ALL LOOP DETECTORS SHALL BE CUT INTO THE FINAL PAVEMENT SURFACE COURSE.
- TRAFFIC SIGNAL CONTROLLERS SHALL BE WIRED SO THAT ANY FIRE PRE-EMPTION SHALL OVERRIDE MANUAL (PUSH BUTTON) OPERATION.
- THE CONTRACTOR SHALL WORK CONTINUOUSLY TO RESTORE TRAFFIC SIGNAL OPERATION TO ITS INTENDED PURPOSE WHEN REPLACING THE TRAFFIC SIGNAL EQUIPMENT. A POLICE DETAIL IS REQUIRED TO DIRECT TRAFFIC AT THE INTERSECTION AT ALL TIMES WHEN THE TRAFFIC SIGNAL IS INOPERATIVE. AT NO TIME SHALL THE CONTRACTOR LEAVE THE SITE BEFORE RESTORING FULL TRAFFIC OPERATIONS.

MAINTENANCE AND PROTECTION OF TRAFFIC NOTES:

- ALL MAINTENANCE AND PROTECTION OF TRAFFIC CONTROL SETUPS, SIGNS, CHANNELIZING DEVICES, ETC., SHALL BE IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION.
- ALL SIGN MOUNTINGS FOR TEMPORARY AND CONSTRUCTION SIGNS SHALL BE IN ACCORDANCE WITH THE R.I.D.O.T. STANDARD SPECIFICATIONS, LATEST EDITION.
- THE CONTRACTOR SHALL COVER ALL EXISTING AND/OR TEMPORARY SIGNS THAT ARE NOT RELEVANT TO THE TRAFFIC CONTROL REQUIRED DURING ANY PARTICULAR STAGE OF THE CONTRACT.
- ADVANCE FLAGPERSON SIGNS (W20-7A) SHALL BE USED IN ADVANCE OF ANY POINT AT WHICH A FLAGPERSON OR A POLICE OFFICER HAS BEEN STATIONED TO CONTROL TRAFFIC. WHEN NEEDED, AN APPROPRIATE DISTANCE MESSAGE MAY BE DISPLAYED ON A SUPPLEMENTAL PLATE (24"x18") BELOW THE FLAGPERSON SYMBOL SIGN. THE SIGN SHALL BE PROMPTLY REMOVED OR COVERED WHENEVER THE FLAGPERSON IS NOT AT THE STATION.
- POLICE OFFICERS (AND NOT FLAGPERSONS) SHALL BE UTILIZED WHEN WORK WILL IMPACT SIGNALIZED INTERSECTIONS AND LIMITED ACCESS HIGHWAYS.
- POLYETHYLENE DRUMS SHALL BE UTILIZED AS A CHANNELIZING DEVICE WHEN A TRAFFIC CONTROL SET-UP IS TO REMAIN BEYOND WORKING HOURS WHEN NO WORKERS ARE PRESENT. CONES SHALL BE UTILIZED WHEN A TRAFFIC CONTROL SET-UP IS TO REMAIN ONLY DURING WORKING HOURS AND IS SUBSEQUENTLY BROKEN DOWN AT THE END OF THE WORKDAY.
- ARROW PANELS SHALL BE SET IN THE FLASHING FOUR CORNERS CAUTION MODE UNLESS UTILIZED FOR A MERGING TAPER. ARROW PANELS SET IN THE FLASHING ARROW MODE SHALL NOT BE UTILIZED FOR LANE SHIFTS.
- TEMPORARY CONSTRUCTION SIGNS AND OTHER WORKZONE TRAFFIC CONTROL DEVICES THAT ARE DAMAGED OR REQUIRE RELOCATION SHALL BE REPLACED AND / OR RELOCATED UNDER THE PAY ITEM FOR "MAINTENANCE AND MOVEMENT TRAFFIC PROTECTION."
- THE PRIVATE VEHICLES OF CONSTRUCTION WORKERS SHALL NOT BE PARKED ON THE TRAVEL LANES OR SHOULDERS. THEY MAY BE PARKED WITHIN THE STATE RIGHT-OF-WAY ONLY IN AREAS 30' BEYOND THE OUTSIDE EDGE OF THE TRAVEL LANES AND/OR IN AREAS APPROVED BY THE ENGINEER.
- TEMPORARY CONSTRUCTION SIGNS AND OTHER TEMPORARY TRAFFIC CONTROL DEVICES SHALL BE INSTALLED PRIOR TO THE START OF WORK IN ANY AREA OPEN TO TRAFFIC, AND SHALL BE REMOVED AS SOON AS PRACTICAL WHEN THEY ARE NO LONGER APPROPRIATE.
- THE INTENDED VEHICLE PATHS THROUGH EACH WORK ZONE SHALL BE CLEARLY MARKED AT ALL TIMES. WATERBORNE PAVEMENT MARKINGS SHALL BE INSTALLED BEFORE THE END OF THE WORK SHIFT ON ALL COLD-PLANNED AND NEW ROADWAY SURFACES THAT WILL BE OPENED TO TRAFFIC AT THE END OF THE SHIFT.

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COMMONWEALTH
ENGINEERS & CONSULTANTS, INC.
400 SMITH STREET
PROVIDENCE, RI 02908



**RHODE ISLAND
DEPARTMENT OF TRANSPORTATION**

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1	4/07	TRB			
2	11/07	TRB			
3	3/10	RBH			
DESIGNED BY:			DATE:		
CHECKED BY:			DATE:		
SCALE: NONE					

**BRIDGE DEMOLITION
OLD SAKONNET RIVER BRIDGE NO. 250**

PORTSMOUTH / TIVERTON RHODE ISLAND

STANDARD NOTES - 2

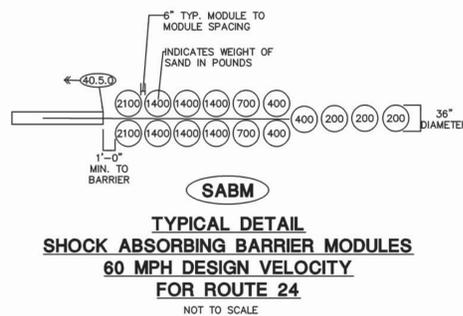
SHEET DESIGNATION
G-3

LEGEND

- RIDOT APPROVED TEMPORARY SIGN SUPPORT
- ◻ UNANCHORED PRECAST CONCRETE BARRIER FOR TEMPORARY TRAFFIC CONTROL STD. 40.5.0
- ▲ FLUORESCENT TRAFFIC CONES STD. 26.1.0
- POLYETHYLENE DRUM BARRICADE WITH MARKINGS STD. 26.2.0 (923.0105). THE MAXIMUM SPACING IS SHOWN ON THE PLANS. A CLOSER SPACING THROUGH THE WORK SPACE MAY BE NEEDED AS DIRECTED BY THE ENGINEER.
- PLASTIC PIPE BARRICADE STD. 26.3.0
- ↑ ADVANCE WARNING ARROW PANEL
- SHOCK ABSORBING BARRIER MODULES
- ⚡ FLAGGER
- ◻ WORK SPACE
- ▨ TRUCK MOUNTED ATTENUATOR WITH FLASHING ARROW BOARD (928.0800)
- ▨ POLICE VEHICLE WITH FLASHING LIGHT BAR

JOB SPECIFIC LEGEND

- CFS COMPOST FILTER SOCK
 - RSGC RESET STOCKPILED GRANITE CURB
 - M PLACEMENT OF MILLING BENEATH GUARDRAIL
 - P-1 4" - CLASS 12.5 HMA
8" GRAVEL BORROW SUBBASE COURSE
 - P-2 3" - CLASS 12.5 HMA
8" GRAVEL BORROW SUBBASE COURSE
 - R&D REMOVE & DISPOSE
 - R&R REMOVE & RESET
 - RDS REMOVE AND DISPOSE SHRUBS
 - S-1 CUTTING AND MATCHING ASPHALT, REFER TO R.I. STD. 47.1.1 (932.0100)
 - S-2 CUTTING AND MATCHING CONCRETE (932.0101)
 - S-3 FULL DEPTH SAWCUT OF BITUMINOUS PAVEMENT (932.0200)
 - SCCB SEDIMENTATION CONTROL - CATCH BASIN
 - SLS REMOVE AND ON-SITE STOCKPILE LIGHT STANDARD AND LUMINAIRE
 - SNOP SNOW FENCE
- 200' CRMC JURISDICTIONAL AREA



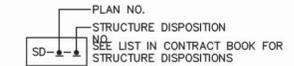
JOB SPECIFIC GENERAL NOTES:

- ANY SUBCONTRACTOR EMPLOYED BY THE CONTRACTOR AND R.I.D.O.T. PERSONNEL ON THIS PROJECT SHALL WORK WITHIN THE SAME PROTECTED WORK AREAS AS THE CONTRACTOR. NO SEPARATE LANE CLOSURES WILL BE PAID FOR.
- TOPOGRAPHIC CONDITIONS ARE OBTAINED FROM AERIAL PHOTOGRAMMETRY. ACCURACY OF INFORMATION IS WITHIN ONE-HALF FOOT.

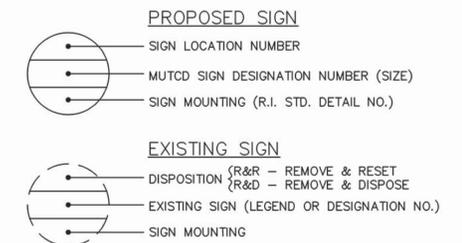
JOB SPECIFIC DRAINAGE AND EROSION CONTROL NOTES:

- ALL DRAINAGE PIPES SHALL BE REINFORCED CONCRETE PIPE (RCP), CLASS III, WITH MORTAR JOINTS UNLESS OTHERWISE NOTED ON THE PLANS. ALL RCP 30" AND LARGER SHALL HAVE "O" RING GASKETS, AND THE JOINTS ON THE OUTSIDE OF THE PIPE SHALL BE MORTARED.
- ALL CATCH BASIN INLET GRATES SHALL HAVE 3--FLANGE FRAMES, UNLESS OTHERWISE NOTED ON THE PLANS.
- ALL R.I. STD. 4.4.0M CATCH BASINS SHALL HAVE A 4--FOOT DIAMETER, UNLESS OTHERWISE NOTED ON THE PLANS.
- THE DRAINAGE SYSTEM SHALL BE INSPECTED AND CLEANED, AS REQUIRED, AFTER THE FINAL RIDING SURFACE HAS BEEN APPLIED AND PRIOR TO ACCEPTANCE OF THE OVERALL PROJECT. THERE SHALL BE NO SEPARATE PAYMENT FOR THIS PROVISION; IT SHALL BE CONSIDERED INCIDENTAL TO OTHER ITEMS OF WORK IN ACCORDANCE WITH SUBSECTION 104.11 OF THE STANDARD SPECIFICATIONS.
- IN ALL CLEAR AND GRUB SLOPE AREAS 2:1 OR STEEPER WHICH WILL REMAIN EXPOSED TO EROSION FOR MORE THAN 2 WEEKS, THE CONTRACTOR SHALL BE RESPONSIBLE TO COVER THESE AREAS WITH FILTER FABRIC AND (R-3) RIP-RAP AT NO ADDITIONAL COST TO THE STATE.
- FOR ALL PROJECTS WITH AT LEAST ONE(1) ACRE OF SOIL DISTURBANCE, R.I.D.O.T. IS REQUIRED TO DEVELOP AND ENFORCE A SITE SPECIFIC STORM WATER POLLUTION PREVENTION PLAN (SWPPP) IN ORDER TO REMAIN IN COMPLIANCE WITH THE RIPDES GENERAL PERMIT FOR STORMWATER DISCHARGES ASSOCIATED WITH CONSTRUCTION ACTIVITIES. THE CONTRACTOR SHALL READ, BECOME FAMILIAR WITH, AND ADHERE TO ALL OF THE PROVISIONS, CONDITIONS, AND STIPULATIONS OF THE GENERAL PERMIT AND THE SITE SPECIFIC SWPPP FOR THIS PROJECT. COPIES OF THESE DOCUMENTS ARE INCLUDED IN THE PROJECT SUPPLEMENTAL DVD. ALL COSTS ASSOCIATED WITH ADHERENCE TO THE SWPPP SHALL BE CONSIDERED INCIDENTAL TO THE CONSTRUCTION AND INCLUDED WITH THE COST FOR THE ASSOCIATED BID ITEM(S).

STRUCTURE DISPOSITIONS



TYPICAL SIGN DESIGNATION SYMBOL

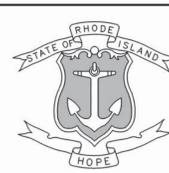


JOB SPECIFIC PLAN SYMBOLS

- | EXISTING | PROPOSED |
|---|----------|
| ○ GCS GAS CURB STOP | ● GCS |
| ○ WCS WATER CURB STOP | ● WCS |
| ○ HH ELECTRIC HANDHOLE | ■ HH |
| ○ SHH SIGNAL HANDHOLE | □ SHH |
| ○ UTILITY TEST PIT (SEE SUBSURFACE UTILITY PLANS) | ■ TP-32 |
| ○ PAVEMENT CORE | ⊕ PC-X-X |

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**RHODE ISLAND
DEPARTMENT OF TRANSPORTATION**

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DESIGNED BY:			DATE:		
CHECKED BY:			DATE:		
SCALE:	NONE				

**BRIDGE DEMOLITION
OLD SAKONNET RIVER BRIDGE NO. 250**

PORTSMOUTH / TIVERTON RHODE ISLAND

**JOB SPECIFIC
PLAN SYMBOLS, LEGEND & NOTES**

SHEET DESIGNATION
G-4

ALL REFERENCES IN THESE GENERAL NOTE SHEETS AND THROUGHOUT THE CONTRACT DRAWINGS TO THE RHODE ISLAND STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION SHALL EXCLUDE THE METHOD OF MEASUREMENT SECTION AND THE BASIS OF PAYMENT SECTION FOR ITEMS PAID FOR UNDER VARIOUS LUMP SUM ITEMS UNDER THIS CONTRACT.

GENERAL NOTES

- ALL CONSTRUCTION INDICATED ON THESE PLANS SHALL BE IN ACCORDANCE WITH:
 - THE 2013 REVISION OF AND SUPPLEMENTS TO THE RHODE ISLAND DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION (RI STANDARD SPECIFICATIONS).
 - THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS (AASHTO) LRFD BRIDGE CONSTRUCTION SPECIFICATIONS, 3rd EDITION DATED 2010, INCLUDING THE LATEST INTERIM REVISIONS TO DATE.
 - THE SPECIFICATIONS ACCOMPANYING THESE PLANS.
- DIMENSIONS, STATIONS, AND ELEVATIONS ARE SHOWN TO THE NEAREST ONE-HUNDREDTH OF A FOOT OR ONE-EIGHTH OF AN INCH, EXCEPT STRUCTURAL STEEL DIMENSIONS WHICH ARE TO THE NEAREST ONE-SIXTEENTH OF AN INCH.
- THE PLANS CONTAINED IN THIS SET WERE GRAPHICALLY PRODUCED FROM PREVIOUS CONSTRUCTION AND SAFETY IMPROVEMENTS PLANS. THE GEOMETRIC INFORMATION, BASELINES, AND ELEVATIONS SHOWN ARE BASED ON THE ORIGINAL DESIGN AND MAY NOT REFLECT THE ACTUAL FIELD CONDITIONS. THE CONTRACTOR SHALL BE RESPONSIBLE TO VERIFY EXISTING CONDITIONS PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE ENGINEER OF ANY DISCREPANCIES IN THE PLANS AND SHALL RECEIVE DIRECTION PRIOR TO PROCEEDING WITH OR CONTINUING THE AFFECTED WORK.
- ANGLES ARE SHOWN TO THE NEAREST SECOND.
- ALL ELEVATIONS ARE REFERENCED TO THE NATIONAL GEODETIC VERTICAL DATUM OF 1929 (NGVD 29)
- COORDINATES USED ON THESE PLANS ARE BASED ON THE STATEWIDE COORDINATE SYSTEM, THE NORTH AMERICAN DATUM OF 1983 (NAD83).
- ALL WORKING POINTS ARE SHOWN AT THE CENTERLINES OF BEARINGS OF ABUTMENTS AND AT THE CENTERLINES OF PIERS, UNLESS OTHERWISE NOTED.
- ALL ABUTMENTS AND WALLS ARE DRAWN LOOKING AT THE EXPOSED FACES.
- ANY DAMAGE TO EXISTING PROPERTY RESULTING FROM THE CONTRACTOR'S OPERATION SHALL BE REPAIRED/REPLACED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE STATE.
- ALL ITEMS NOT REFERENCED FOR REMOVAL OR MODIFICATION WILL BE "EXISTING TO REMAIN" UNLESS OTHERWISE DIRECTED BY THE ENGINEER. THE CONTRACTOR SHALL TAKE SPECIAL CARE NOT TO DAMAGE PORTIONS OF THE EXISTING STRUCTURE TO REMAIN. IF DAMAGE OCCURS TO PORTIONS OF THE EXISTING STRUCTURE DESIGNATED TO REMAIN, BY THE CONTRACTOR'S OPERATIONS, THE CONTRACTOR WILL BE REQUIRED TO REPAIR OR REPLACE THE DAMAGED PORTIONS, AS DETERMINED BY THE ENGINEER AT THE CONTRACTOR'S EXPENSE.
- THE EXISTING UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE AND WERE LOCATED USING THE BEST AVAILABLE INFORMATION. NO BUILDING SERVICE CONNECTIONS (ELECTRIC, TELEPHONE, GAS, WATER, SANITARY AND OTHERS) ARE SHOWN. THE CONTRACTOR SHALL ASSUME THAT SERVICES TO ALL BUILDINGS ARE PRESENT.
- BOTH FEDERAL AND STATE LAW (RI. GENERAL LAW 39-1.2) REQUIRE NOTIFICATION OF APPROPRIATE UTILITY COMPANIES BEFORE DIGGING, TRENCHING, BLASTING, DEMOLISHING, BORING, BACK FILLING, GRADING, LANDSCAPING, OR OTHER EARTH MOVING OPERATIONS. IT IS THE CONTRACTOR'S RESPONSIBILITY TO NOTIFY ALL UTILITY COMPANIES (INCLUDING THROUGH THE "DIG SAFE" PROGRAM) TO ENSURE THAT ALL UTILITIES, BOTH UNDERGROUND AND OVERHEAD, HAVE BEEN MARKED BEFORE COMMENCEMENT OF SUCH WORK. THE CONTRACTOR SHOULD UNDERSTAND THAT NOT ALL UTILITIES SUBSCRIBE TO THE "DIG SAFE" PROGRAM. ANY DAMAGE TO EXISTING UTILITIES MARKED IN THE FIELD, OR AS A RESULT OF FAILING TO CONTACT THE APPROPRIATE UTILITY COMPANIES, SHALL BE REPAIRED OR REPLACED (AS DEEMED APPROPRIATE BY THE STATE AND/OR THE IMPACTED UTILITY COMPANY) AT NO ADDITIONAL COST TO THE STATE.
- THIS PROJECT IS ON A HURRICANE EVACUATION AND DIVERSIONARY ROUTE. THE CONTRACTOR IS ADVISED THAT UPON 12 (TWELVE) HOURS NOTICE THE ROADWAYS SHALL BE OPEN TO EVACUEES AND EMERGENCY PERSONNEL. ANY EXTRA WORK NECESSARY TO COMPLY WITH THIS REQUIREMENT WILL BE REIMBURSED UNDER FORCE ACCOUNT PROCEDURES.

DESIGN DATA

DESIGN SPECIFICATIONS

- THE AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, SEVENTH EDITION DATED 2014, INCLUDING ALL INTERIM REVISIONS TO DATE.
- THE RHODE ISLAND LRFD BRIDGE DESIGN MANUAL DATED 2007.
- ALL OTHER APPLICABLE DESIGN SPECIFICATIONS ARE REFERENCED IN SECTION 1 OF THE RHODE ISLAND LRFD BRIDGE DESIGN MANUAL DATED 2007.
- THE 2013 REVISION OF AND SUPPLEMENTS TO THE RHODE ISLAND DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION (RI STANDARD SPECIFICATIONS).
- IN CASE OF CONFLICT, THE RHODE ISLAND LRFD BRIDGE DESIGN MANUAL SHALL GOVERN.

CONSTRUCTION ACCESS

- THE CONTRACTOR SHALL NOTE THE LIMITED ACCESSIBILITY AVAILABLE TO CONDUCT THE VARIOUS WORK ITEMS PROPOSED.
- ALL WORK SHALL BE CONDUCTED WITHIN THE LIMITS OF DISTURBANCE SHOWN ON THESE PLANS.
- PROPERTY OWNER ACCESS TO ADJACENT PRIVATE PROPERTIES SHALL BE MAINTAINED AT ALL TIMES. THE CONTRACTOR SHALL CONDUCT ALL NECESSARY COORDINATION WITH PROPERTY OWNERS AS REQUIRED TO PROVIDE ACCESS AND MINIMIZE DISTURBANCE DURING WORKING HOURS.

UTILITIES

- LOCATION OF EXISTING UTILITIES SHOWN ARE APPROXIMATE. THE CONTRACTOR SHALL VERIFY LOCATIONS OF ALL UTILITIES, SHOULD IT AFFECT ANY CONSTRUCTION OPERATIONS. THE "DIG SAFE" POLICY SHALL ALSO BE FOLLOWED. ANY DAMAGE TO EXISTING UTILITIES SHOWN ON THE PLANS OR LOCATED BY DIG SAFE SHALL BE THE CONTRACTOR'S RESPONSIBILITY. COST OF DAMAGE TO THESE UTILITIES SHALL BE BORNE BY THE CONTRACTOR.
- THE CONTRACTOR IS ADVISED OF THE PRESENCE OF ELECTRICAL SERVICE CONDUIT IN THE MEDIAN AS WELL AS IN THE UPPER SUPERSTRUCTURE OF THE "THROUGH-TRUSS" PORTION OF THE MAIN SPAN AND ON BENTS 15 & 16. THE CONTRACTOR SHALL COORDINATE OPERATIONS WITH THE APPROPRIATE UTILITY COMPANIES TO CONFIRM THAT THESE LINES HAVE BEEN DEACTIVATED.
- NAVIGATIONAL LIGHTING MUST REMAIN IN SERVICE AT ALL TIMES.
- OVERHEAD WIRES EXIST IN THE VICINITY OF THE STRUCTURE ON BOTH SIDES OF THE BRIDGE. PRIOR TO THE COMMENCING OF ANY OF HIS OPERATIONS, THE CONTRACTOR SHALL CONFIRM THE LOCATIONS OF THESE WIRES AND MAINTAIN AWARENESS OF THEM THROUGH THE DURATION OF HIS WORK.
- FIBER-OPTIC / TELEVISION CABLE ALSO EXISTS AT VARIOUS LOCATIONS ON THE STRUCTURE, INCLUDING, BUT NOT LIMITED TO BENT 15 / 16, BENT 5 AND SOUTH FASCIA. THE CONTRACTOR SHALL BE RESPONSIBLE TO CONFIRM THAT THESE LINES HAVE BEEN DEACTIVATED.

GENERAL NOTES REGARDING TEMPORARY CONSTRUCTION CONDITIONS:

- DESIGN WIND PRESSURES FOR CONSTRUCTION:

WIND PRESSURES TO BE USED BY THE CONTRACTOR DESIGN DURING THE CONSTRUCTION CONTRACT (WITH THE EXCEPTION OF SIGNS) SHALL BE FROM THE FOLLOWING TABLE :

HEIGHT ABOVE GROUND (FEET)	WIND PRESSURE (PSF)
UP TO 17'	33
OVER 17' AND UP TO 33'	37
OVER 33' AND UP TO 50'	41
OVER 50' AND UP TO 75'	44
OVER 75' AND UP TO 100'	47

TABLE NOTES:

- APPLICATION OF THE TABULAR PRESSURES:
 - BRIDGE COMPONENTS DURING CONSTRUCTION, PRIOR TO THE INSTALLATION OF THE PERMANENT BRACING SYSTEMS, NOT INCLUDING CRANE LIFTING,
 - FALSEWORK, SHORING, AND SCAFFOLDING AS DEFINED IN FHWA "GUIDE DESIGN SPECIFICATION FOR BRIDGE TEMPORARY WORKS", EXCLUDING 3-DIMENSIONAL LATTICED OR TRUSSED FRAMES OR TOWERS,
 - TEMPORARY SHIELDING
- WIND PRESSURES FOR ALL OTHER STRUCTURES SHALL BE CALCULATED BASED ON ASCE "DESIGN LOADS ON STRUCTURES DURING CONSTRUCTION", SEI/ASCE 37-02. THE EXPOSURE CATEGORY SHALL BE C.
- FOR STRUCTURES SITUATED ABOVE LIVE INTERSTATE TRAFFIC, THE ABOVE VALUES SHALL BE INCREASED BY 5 PSF.

- DEMOLITION OF EXISTING BRIDGE COMPONENTS:

FOR THE DEMOLITION OF THE OLD SAKONNET RIVER BRIDGE, REFER TO THE BRIDGE DEMOLITION NOTES ON "DEMOLITION NOTES & DETAILS" SHEET.

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**RHODE ISLAND
DEPARTMENT OF TRANSPORTATION**

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DESIGNED BY:			DATE:		
CHECKED BY:			DATE:		
SCALE:	NONE				

BRIDGE DEMOLITION OLD SAKONNET RIVER BRIDGE NO. 250	
PORTSMOUTH / TIVERTON	RHODE ISLAND
BRIDGE GENERAL NOTES - 1	SHEET DESIGNATION G-5

FED. ROAD DIV. NO.	STATE	FEDERAL AID PROJECT NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
	RI			7	33

WORK RESTRICTIONS DUE TO PEREGRINE FALCON NESTING

- PEREGRINE FALCONS (STATE OF RHODE ISLAND THREATENED SPECIES) ARE KNOWN TO NEST ON THE OLD SAKONNET RIVER BRIDGE. KNOWN PEREGRINE FALCON NESTING LOCATIONS ARE INDICATED ON THE PLANS.
- IF, THE DEPARTMENT CONFIRMS THAT THE PEREGRINE FALCONS HAVE SELECTED A NESTING SITE (OR SITES) ON THE BRIDGE THEN NO WORK IS PERMITTED WITHIN 150 FEET OF THE NESTING SITE UNTIL THE YOUNG BIRDS HAVE FLEDGED.
- IN ADDITION, SHOULD THE CONTRACTOR ENCOUNTER A PEREGRINE FALCON NESTING SITE NOT INDICATED ON THE PLANS OR IDENTIFIED BY THE DEPARTMENT, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE ENGINEER, AND WORK WITHIN 150 FEET OF THE NEW NESTING SITE SHALL CEASE UNTIL THE CONTRACTOR RECEIVES FURTHER DIRECTION FROM THE DEPARTMENT.
- UNDER NO CIRCUMSTANCES WILL THE CONTRACTOR OR HIS PERSONNEL PHYSICALLY HARM, KILL, DISTURB, HARASS, OR AGITATE A PEREGRINE FALCON IN ANY MANNER

DESIGN TIDAL INFORMATION

MEAN HIGH HIGH WATER ELEVATION	=	+2.67'
MEAN HIGH WATER ELEVATION	=	+2.40'
NGVD 1929 ELEVATION	=	0.00'
MEAN LOW WATER ELEVATION	=	-1.35'
MEAN LOW LOW WATER ELEVATION	=	-1.53'

THE CONTRACTOR SHALL NOTE THAT HIGHER AND LOWER TIDES ARE POSSIBLE.

TEMPORARY PROTECTION TO PUBLIC WHEN WORKING ABOVE THE ROADWAY OR RIVER:

- WHEN WORKING ABOVE THE RIVER OR ROADWAY, THE CONTRACTOR SHALL INSURE THAT NO, DEBRIS, TOOLS, EQUIPMENT OR MATERIAL ARE PERMITTED TO FALL TO THE RIVER OR ROADWAY BELOW THE BRIDGE.
- THE CONTRACTOR SHALL PROVIDE THE FOLLOWING MINIMUM TEMPORARY PROTECTIVE MEASURES DURING THE DEMOLITION OF THE EXISTING REINFORCED CONCRETE BRIDGE DECK:
 - PRIOR TO DEMOLITION THE CONTRACTOR SHALL INSTALL UNDER DECK PROTECTION, COMPRISING DEBRIS NETTING AND/OR SHIELDING BELOW ALL SPANS.
- DURING THE REMOVAL OF THE BRIDGE DECK ABOVE DEPOT AVE. AND RIVERSIDE DRIVE THE CONTRACTOR SHALL BE RESPONSIBLE TO DETOUR TRAFFIC IN ACCORDANCE WITH THE REQUIREMENTS OF THE CONTRACT DOCUMENTS.
- DURING THE REMOVAL OF THE BRIDGE DECK ABOVE THE RIVER THE CONTRACTOR SHALL BE RESPONSIBLE TO REDIRECT MARINE TRAFFIC IN ACCORDANCE WITH THE REQUIREMENTS OF THE CONTRACT DOCUMENTS, AND PER THE DIRECTION OF THE USCG AND RIDEM.
- DURING THE REMOVAL OF THE STEEL SUPERSTRUCTURE OVER ROADWAYS AND BENTS ADJACENT TO ROADWAYS THE CONTRACTOR SHALL PROVIDE A FLAGGER TO TEMPORARILY BLOCK PUBLIC ACCESS AS NECESSARY TO ENSURE THE SAFE PASSAGE OF PEDESTRIANS AND TRAFFIC.
- DURING THE REMOVAL OF THE STEEL SUPERSTRUCTURE OVER THE RIVER THE CONTRACTOR SHALL BE RESPONSIBLE TO REDIRECT MARINE TRAFFIC IN ACCORDANCE WITH THE REQUIREMENTS OF THE CONTRACT DOCUMENTS, AND PER THE DIRECTION OF THE USCG AND RIDEM.
- DURING THE REMOVAL OF THE STEEL SUPERSTRUCTURE AND BENTS THE CONTRACTOR SHALL BE RESPONSIBLE TO PREVENT PUBLIC ACCESS IN THE WORK ZONE IN ACCORDANCE WITH THE REQUIREMENTS OF THE CONTRACT DOCUMENTS, AND THE CONTRACTOR'S APPROVED WORK PLAN. THE CONTRACTOR'S WORK PLAN WILL BE SUBJECT TO USCG AND RIDEM APPROVAL.
- THE ABOVE DESCRIBED MEASURES ARE THE MINIMUM REQUIREMENTS WHEN UTILIZING CONVENTIONAL MECHANICAL METHODS FOR THE DEMOLITION OF THE BRIDGE.

DEBRIS NETTING AND SHIELDING:

- THE CONTRACTOR SHALL PROVIDE UNDER-DECK PROTECTION, COMPRISING OF DEBRIS NETTING AND/OR TEMPORARY DECK UNDERSIDE AND SIDE PROTECTIVE SHIELDING UNDER ALL AREAS OF BRIDGE DECK REMOVAL, ALL IN ACCORDANCE WITH THE CONTRACT DOCUMENTS AND THESE SPECIAL PROVISIONS, AND AS DIRECTED BY THE ENGINEER.
- THE CONTRACTOR MAY USE A COMBINATION OF DEBRIS NETTING AND TEMPORARY SHIELDING OVER THE LENGTH OF THE BRIDGE AT HIS DISCRETION, PROVIDED THAT ALL AREAS TO BE PROTECTED CONFORM TO THE REQUIREMENTS OF THIS SPECIFICATION AND ALL OTHER CONTRACT DOCUMENTS.
- THE UNDER-DECK PROTECTION SHALL PREVENT ANY DEBRIS FROM FALLING TO THE GROUND OR WATER, PROVIDE FOR THE SAFE PASSAGE OF VEHICLES, MARINE VESSELS, PEDESTRIANS, MARINE TRAFFIC AND PROVIDE PROTECTION FOR UTILITIES. THE USE OF THIS UNDER-DECK PROTECTION IS MANDATORY.
- THE COST FOR THIS WORK SHALL BE INCLUDED FOR PAYMENT UNDER THE DEMOLITION ITEMS INCLUDED IN THE CONTRACT. NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM.

TEMPORARY CONSTRUCTION ACCESS NOTES

- THE CONTRACTOR IS SOLELY RESPONSIBLE FOR THE DESIGN AND PERFORMANCE OF ALL TEMPORARY SUPPORTS FOR THE DEMOLITION OF THE BRIDGE AS PART OF HIS MEANS AND METHODS. THE CONTRACTOR IS ALSO SOLELY RESPONSIBLE FOR THE DESIGN AND PERFORMANCE OF TEMPORARY SUPPORT FOR ALL EXISTING STRUCTURES, ROADWAYS AND UTILITIES DURING CONSTRUCTION. ALL TEMPORARY SUPPORTS ARE SUBJECT TO REVIEW AND APPROVAL BY THE RHODE ISLAND COASTAL RESOURCES MANAGEMENT COUNCIL (CRMC), THE RHODE ISLAND DEPARTMENT OF ENVIRONMENT MANAGEMENT (RIDEM), THE UNITED STATES COAST GUARD (USCG), AND THE UNITED STATES ARMY CORPS OF ENGINEERS (ACOE). NO TEMPORARY SUPPORTS WILL BE PERMITTED TO BE ERECTED PRIOR TO APPROVAL FROM ALL FOUR AGENCIES.
- THE CONTRACTOR SHALL SUBMIT A DESIGN STAMPED BY A PROFESSIONAL ENGINEER REGISTERED IN THE STATE OF RHODE ISLAND FOR ALL TEMPORARY SUPPORTS TO BE USED FOR CONSTRUCTION ACCESS. IN ADDITION TO DRAWINGS AND CALCULATIONS, THE CONTRACTOR'S DESIGN SUBMITTAL SHALL PROVIDE A DETAILED CONSTRUCTION SEQUENCE. WRITTEN APPROVAL OF THE SUBMITTAL BY THE ENGINEER SHALL BE OBTAINED BY THE CONTRACTOR PRIOR TO COMMENCING THE WORK.
- THE CONTRACTOR SHALL SUBMIT A WORK PLAN TO THE ENGINEER FOR SUBMISSION TO THE ENVIRONMENTAL PERMIT AGENCIES FOR APPROVAL PRIOR TO STARTING WORK. WORK PLAN MUST COMPLY WITH THE USCG REQUIREMENTS AND THE STIPULATIONS OF THE ENVIRONMENTAL PERMIT FOUND IN THE CS PAGES OF THE CONTRACT DOCUMENTS.
- THE CONTRACTOR SHALL ANTICIPATE THAT SOME AREAS OF THE TEMPORARY SUPPORT OF EXCAVATION ARE LOCATED IN THE INTERTIDAL ZONE AND SHALL NOT BE READILY ACCESSIBLE BY FLOATING EQUIPMENT OR BY LAND USING CONVENTIONAL CONSTRUCTION EQUIPMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR DEVELOPING HIS OWN PROCEDURES FOR ACCESS TO INSTALL THIS TEMPORARY SUPPORT OF EXCAVATION. CONTRACTOR'S SPECIAL PROCEDURES SHALL CONFORM TO THE INTENT OF THE REQUIREMENTS OF THE ENVIRONMENTAL PERMIT STIPULATIONS FOUND IN THE CS PAGES OF THE CONTRACT DOCUMENTS.

DESIGN CRITERIA

THE CONTRACTOR'S DESIGN CALCULATIONS SHALL ADHERE TO THE FOLLOWING:

- GEOTECHNICAL SOIL PARAMETERS USED IN DESIGN OF TEMPORARY EARTH SUPPORT STRUCTURES SHALL REFLECT THE SUBSURFACE CONDITIONS SHOWN IN THE BORINGS WITH DUE CONSIDERATION FOR THE POSSIBILITY OF VARIATIONS IN SUBSURFACE CONDITIONS BETWEEN EXPLORATION LOCATIONS.
- IF THE EXISTING SUBSURFACE DATA IS INSUFFICIENT FOR THE PURPOSES OF THIS CONTRACT, THE CONTRACTOR SHALL BE RESPONSIBLE FOR DETERMINING HIS OWN SUBSURFACE EXPLORATION NEEDS AS NECESSARY FOR SUPPORTING ITS DESIGN OF TEMPORARY WORKS. IF IMPLEMENTED BY THE CONTRACTOR, SOIL BORINGS SHALL BE PERFORMED AND PAID FOR UNDER THE APPROPRIATE UNIT BID ITEM PROVIDED UNDER THIS CONTRACT.
- DIFFERENTIAL WATER, CURRENT, AND WAVE LOADS DUE TO TIDAL FLUCTUATIONS AND STORM EVENTS SHALL BE CONSIDERED IN THE DESIGN.
- SURCHARGE LOADING ASSUMED IN TEMPORARY EARTH SUPPORT DESIGN SHALL MEET OR EXCEED LOADING IMPOSED BY THE CONTRACTOR'S EQUIPMENT AT EACH STAGE OF CONSTRUCTION AND FOR THE DURATION OF CONSTRUCTION. CONTRACTOR SHALL MAKE HIS OWN ASSESSMENT OF ALLOWABLE TEMPORARY SURCHARGE LOADING FOR ALL SHEETPILE STRUCTURES AND STAGES OF CONSTRUCTION.

TEMPORARY FILL AND TEMPORARY STRUCTURES:

- CONTAINMENT BARRIERS MUST BE INSTALLED PRIOR TO THE INSTALLATION OF ANY TEMPORARY SUPPORT OF EXCAVATION STRUCTURES.
- THE LIMITS FOR THE TEMPORARY CONTAINMENT BARRIERS ARE DEPICTED ON THESE PLANS. THESE LIMITS REPRESENT THE MAXIMUM LIMITS OF DISTURBANCE PERMITTED FOR THIS PROJECT.
- TEMPORARY FILL IF PLACED SHALL BE CRUSHED ROCK FILL MEETING THE REQUIREMENTS OF SECTIONS M.01.05.4 AND M.01.09 TABLE 1, COLUMN II OF THE RHODE ISLAND STANDARD SPECIFICATIONS. THE CONTRACTOR SHALL SUBMIT DOCUMENTATION THAT THE PROPOSED TEMPORARY FILL MEETS THE REQUIREMENTS FOR CRUSHED ROCK FILL.
- THE CONTRACTOR SHALL BE RESPONSIBLE TO DESIGN AND DETERMINE AN APPROPRIATE SYSTEM TO SATISFY HIS SELECTED MEANS AND METHODS AND MEET THE CONDITIONS OF THE CONTRACT DOCUMENTS AND THE ENVIRONMENTAL PERMITS.

SITE RESTORATION

- THE CONTRACTOR SHALL REMOVE THE CONTRACTOR-DESIGNED TEMPORARY SUPPORT OF EXCAVATION IN THEIR ENTIRETY, WHEN NO LONGER NEEDED FOR ITS OPERATIONS. REMOVAL WORK SHALL BE CONDUCTED WITHIN A TEMPORARY SILT CURTAIN. ALL TEMPORARY STRUCTURES SHALL BE REMOVED AND LEGALLY DISPOSED OFFSITE.
- EXISTING GRADES SHOWN ON THE DRAWINGS WITHIN THE LIMITS OF THE SAKONNET RIVER INCLUDE NATURAL RIVER BOTTOM GRADES. THE WORK TO BE COMPLETED AS PART OF THE CONSTRUCTION ACCESS REQUIRES RIVER BOTTOM RESTORATION TO ONLY THE EXISTING GRADES. ALL TEMPORARY FILL SHALL BE REMOVED AND LEGALLY DISPOSED OFFSITE.
- FOLLOWING REMOVAL OF ANY TEMPORARY ACCESS STRUCTURES AND ALL PLACED FILL THE CONTRACTOR SHALL SUBMIT AS-BUILT DRAWINGS SHOWING FINAL GRADES IN THE CONSTRUCTION ACCESS AREA AND PHOTOGRAPHIC DOCUMENTATION SHOWING THAT ALL TEMPORARY FILL HAS BEEN REMOVED AND THE SAKONNET RIVER BOTTOM RESTORED. THE DRAWINGS SHALL BE AN AUTOCAD DRAWING, SUBMITTED BOTH ELECTRONICALLY AND BY HARD COPY.
- PUBLIC OR PRIVATE PROPERTY WITHIN OR ADJACENT TO THE PROJECT LIMITS, WHICH IS DISTURBED BY THE CONTRACTOR'S OPERATIONS, SHALL BE RESTORED TO ITS PRE-CONSTRUCTION CONDITION BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER. ALL RESTORATION SHALL BE IN ACCORDANCE WITH THE APPLICABLE PROVISIONS OF THE RHODE ISLAND STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, AMENDED 2013. THE COST OF RESTORATION SHALL BE INCLUDED IN THE PRICES BID FOR OTHER SCHEDULED ITEMS OF WORK AND NO SEPARATE PAYMENT SHALL BE MADE.

LUMP SUM BID ITEMS:

- THE CONTRACTOR SHALL NOTE THAT THE NUMEROUS BRIDGE ITEMS ON THIS PROJECT ARE PAID FOR ON A LUMP SUM BASIS OR ARE INCLUDED FOR PAYMENT UNDER OTHER LUMP SUM ITEM(S). IN GENERAL THESE INCLUDE BUT ARE NOT LIMITED TO:
 - THE DEMOLITION OF PORTIONS OF THE OLD SAKONNET RIVER BRIDGE.
 - THE CLEANING OF HORIZONTAL SURFACES OF THE WEST ABUTMENT, CONCRETE PIERS AND BENT CONCRETE FOOTINGS.
 - THE REMOVAL AND SALVAGING OF THE EXISTING ELECTRICAL SYSTEM ON THE OLD BRIDGE 250.
 - TEMPORARY NAVIGATION LIGHTING.
 - PHOTO & VIDEO OF DEMOLITION OPERATIONS.
- IN ACCORDANCE WITH SECTIONS 109.07 "PARTIAL PAYMENT OF LUMP SUM ITEMS", WITHIN TEN (10) CALENDAR DAYS AFTER THE DATE OF NOTICE OF AWARD, THE CONTRACTOR SHALL SUBMIT TO THE ENGINEER FOR APPROVAL THE BREAKDOWN OF EACH LUMP SUM BID ITEM.

SPECIAL NOTES TO CONTRACTOR:

- THE CONTRACTOR SHALL BE AWARE OF THE OVERHEAD HIGH VOLTAGE ELECTRICAL TRANSMISSION LINES WITHIN THE PROJECT LIMITS. THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS WHEN WORKING AROUND THESE LINES AND COMPLY WITH ALL CURRENT OSHA SAFETY AND UTILITY COMPANY REQUIREMENTS.

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**RHODE ISLAND
DEPARTMENT OF TRANSPORTATION**

REVISIONS			REVISIONS		
NO.	DATE	BY	NO.	DATE	BY
DESIGNED BY:			DATE:		
CHECKED BY:			DATE:		
SCALE:	NONE				

BRIDGE DEMOLITION OLD SAKONNET RIVER BRIDGE NO. 250	
PORTSMOUTH / TIVERTON	RHODE ISLAND
BRIDGE GENERAL NOTES - 2	SHEET DESIGNATION G-6

LIST OF ABBREVIATIONS

A
 ABUTMENT = ABUT.
 ADDITIONAL = ADD'L
 ALTERNATE = ALT.
 ANCHOR BOLT = A.B.
 AND = &
 APPROACH = APPR.
 APPROVED = APPD.
 APPROXIMATE = APPROX.
 AT EACH = @
 AVENUE = AVE.
 AVERAGE = AVG.

B
 BACK TO BACK = B TO B
 BASELINE = B
 BEAM = BM.
 BETWEEN = BTWN
 BEARING = BRG.
 BITUMINOUS = BIT.
 BUILDING = BLDG.
 BUILDING LINE = B.L.
 BOLT CIRCLE = B.C.
 BOTTOM = BOT.
 BOTTOM OF = B.O.

C
 CENTER TO CENTER = C TO C
 CENTERLINE = C
 CIRCLE = CIR.
 CLEARANCE = CL.
 COLUMN = COL.
 CONCRETE = CONC.
 CONDUIT = COND.
 CONNECTION = CONN.
 CONSTRUCTION = CONST.
 CONTRACTION = CONTR.
 COUNTERSINK = CSK.
 COUPLING = CPLG.
 CLASS I CONTROLLED LOW STRENGTH MATERIAL = CLSM
 CUBIC FEET = C.F.

D
 DETAIL = DET.
 DIAGONAL = DIAG.
 DIAPHRAGM = DIAPHM.
 DIAMETER = DIA.
 DIMENSION = DIM. OR ϕ
 DOWN = DN.
 DRAWING = DWG.
 DRAIN = DR.

E
 EACH = EA.
 EACH FACE = E.F.
 EAST = E.
 ELEVATION = EL. OR ELEV.
 EMBEDMENT = EMBED.
 EXISTING = EXIST.
 EXPANSION = EXP.
 EQUAL = EQ.

F
 FAR FACE = F.F.
 FAR SIDE = F.S.
 FABRICATE = FAB.
 FACE TO FACE = F TO F
 FEET = FT.
 FLANGE = FLG.
 FLAT HEAD = F.H.
 FOOTING = FTG.
 FORCE MAIN = FM
 FOUNDATION = FDN.
 FURNISH, FABRICATE & ERECT = F.F. & E.

G
 GAGE = GA.
 GALVANIZE = GALV.
 GAS = G
 GRADE = GR.
 GRATING = GRTG.
 GROUND = GND.

H
 HEIGHT = HGT.
 HEXAGON = HEX.
 HIGHWAY = HWY.
 HIGH STRENGTH = H.S.
 HORIZONTAL = HORIZ.

I
 INCH = IN.
 INFORMATION = INFO.
 INSIDE DIAMETER = I.D.
 INVERT = INV.

J
 JOINT = JT.

L
 LENGTH = LGTH. OR L
 LENGTH OF VERTICAL CURVE = L.V.C.
 LEFT = LT.
 LIGHTING = LTG.
 LONG = LG.
 LOAD & RESISTANCE FACTOR DESIGN = LRFD

M
 MATERIAL = MATL.
 MAXIMUM = MAX.
 MEAN HIGH WATER = M.H.W.
 MEAN LOW WATER = M.L.W.
 MEAN SEA LEVEL = M.S.L.
 MECHANICAL = MECH.
 MINIMUM = MIN.
 MISCELLANEOUS = MISC.

N
 NEAR FACE = N.F.
 NEAR SIDE = N.S.
 NORTH = N.
 NOT TO SCALE = N.T.S.
 NUMBER = NO. OR #

O
 OBSERVED WATER = O.W.
 ON CENTER = O.C.
 OPENING = OPNG.
 OUTSIDE DIAMETER = O.D.
 OPTIONAL = OPT.

P
 PLATE = P
 PLUS OR MINUS = \pm
 POINT OF VERTICAL CURVATURE = P.V.C.
 POINT OF VERTICAL INTERSECTION = P.V.I.
 POINT OF VERTICAL TANGENCY = P.V.T.
 POLYVINYL CHLORIDE = PVC
 POINT OF TANGENCY = P.T.
 POUNDS = LBS
 POUNDS PER SQUARE INCH = P.S.I.
 POUNDS PER SQUARE FOOT = P.S.F.
 PRESTRESSED PRECAST CONCRETE = P.P.C.
 PRECAST CONCRETE = P.C.

R
 RADIUS = RAD. OR R
 RAILROAD = RR
 REQUIRED = REQD.
 REINFORCING = REINF.
 REHABILITATION = REHAB.
 REMOVE & DISPOSE = R & D
 RIGHT = RT.

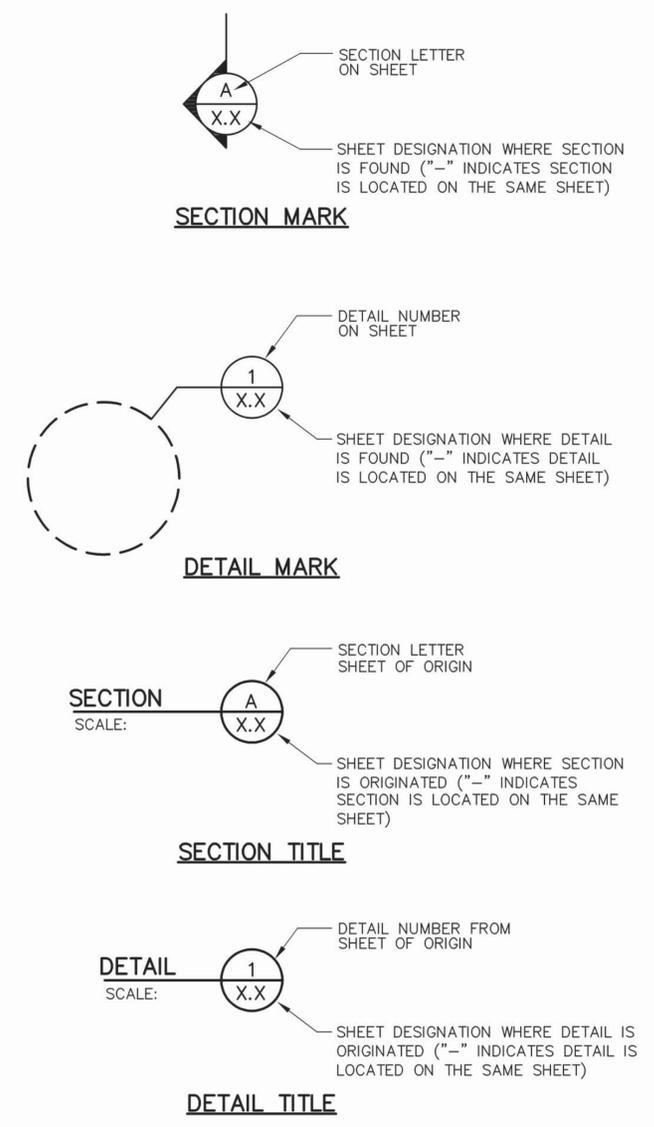
S
 SECTION = SECT.
 SCHEDULE = SCH.
 SCHEMATIC = SCHEM.
 SHEET = SH.
 SIDEWALK = S.W.
 SOUTH = S.
 SPACES = SP.
 STANDARD = STD.
 STATION = STA.
 SYMMETRICAL = SYM.
 STAY IN PLACE = S.I.P.
 SQUARE = SQ.

T
 TOP = T
 TOP AND BOTTOM = T&B
 TOP OF = T.O.
 THICK = THK.
 TYPICAL = TYP.

U
 UNLESS NOTED OTHERWISE = U.N.O.

V
 VARIES = VAR.
 VERTICAL CURVE = V.C.
 VERTICAL = VERT.

W
 WATER = W
 WELDED WIRE FABRIC = W.W.F.
 WEST = W.
 WITH = W/
 WIDE FLANGE = W.F.
 WORKING POINT = W.P.



SECTION & DETAIL DESIGNATIONS

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RHODE ISLAND
 DEPARTMENT OF TRANSPORTATION

REVISIONS			REVISIONS		
NO.	DATE	BY	NO.	DATE	BY

DESIGNED BY: _____ DATE: _____
 CHECKED BY: _____ DATE: _____
 SCALE: NONE

BRIDGE DEMOLITION
OLD SAKONNET RIVER BRIDGE NO. 250
 PORTSMOUTH / TIVERTON
 RHODE ISLAND

LIST OF ABBREVIATIONS & PLAN DESIGNATIONS

SHEET DESIGNATION
G-7

FED. ROAD DIV. NO.	STATE	FEDERAL AID PROJECT NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
	RI			9	33

BRIDGE DEMOLITION NOTES:

1. THE REMOVAL OF THE SPECIFIED PORTIONS OF EXISTING SAKONNET RIVER BRIDGE SHALL BE PERFORMED BY MECHANICAL METHODS ONLY.
2. THE SAKONNET RIVER IS A NAVIGABLE WATERWAY.
3. THE CONTRACTOR SHALL SUBMIT PLANS FOR EACH STAGE OF THE BRIDGE SUPERSTRUCTURE AND STEEL BENT DEMOLITION PROCEDURE. THIS SUBMISSION SHALL INCLUDE COMPLETE DETAILS (WITH DRAWINGS, CALCULATIONS, DEMOLITION METHODS, EQUIPMENT, CREW SIZES, SEQUENCE, DURATIONS) OF THE PROCEDURE INCLUDING, BUT NOT LIMITED TO, TEMPORARY SUPPORTS, SCHEDULING AND OPERATION SEQUENCING, CRANE PLACEMENT, LOAD AND STRESS COMPUTATIONS DURING THE VARIOUS STAGES OF LIFTING. THIS APPLIES TO THE EXISTING BRIDGE AS WELL AS ALL TEMPORARY STRUCTURES. THE CAPACITY OF THE CRANE AND ALL LIFTING AND CONNECTING DEVICES SHALL BE ADEQUATE FOR 125% OF THE TOTAL PICK LOAD INCLUDING SPREADERS AND ALL OTHER MATERIALS. THIS FACTOR OF SAFETY SHALL BE IN ADDITION TO ALL MANUFACTURERS' PUBLISHED FACTORS OF SAFETY. THE DEMOLITION PROCEDURE IS SUBJECT TO REVIEW AND APPROVAL BY CRMC, RIDEM, USCG, AND ACOE. NO BRIDGE DEMOLITION ACTIVITIES WILL BE PERMITTED PRIOR TO APPROVAL FROM ALL FOUR AGENCIES.
4. THE CONTRACTOR WILL NOT BE ALLOWED TO PROCEED WITHOUT AN APPROVED DEMOLITION PROCEDURE.
5. A REGISTERED PROFESSIONAL ENGINEER, LICENSED IN THE STATE OF RHODE ISLAND, WILL BE REQUIRED TO STAMP THE CONTRACTOR'S DEMOLITION SHOP DRAWING PLANS AND PROCEDURES.
6. ANY CHANGES TO THE DEMOLITION PLANS MUST BE STAMPED BY THE CONTRACTOR'S PROFESSIONAL ENGINEER, LICENSED IN THE STATE OF RHODE ISLAND, AND SUBMITTED TO RIDOT FOR REVIEW. ALL CHANGES ARE SUBJECT TO REVIEW AND APPROVAL BY CRMC, RIDEM, USCG, AND ACOE AND MUST BE APPROVED BY RIDOT PRIOR TO STARTING THE WORK.
7. A MANDATORY PRE-CONSTRUCTION MEETING WILL BE HELD A MINIMUM OF TWO WEEKS PRIOR TO THE START OF DEMOLITION TO DISCUSS THE PLAN AND PROCEDURES, WORK SCHEDULES, CONTINGENCY PLANS, SAFETY REQUIREMENTS AND TRAFFIC (VEHICULAR AND BOAT) CONTROL. THE CONTRACTOR'S PROFESSIONAL ENGINEER AND APPLICABLE SUBCONTRACTORS WILL BE REQUIRED TO ATTEND THIS MEETING, AS WILL THE RIDOT RESIDENT ENGINEER, THE DESIGN PROJECT ENGINEER, AND THE DESIGN CONSULTANT.
8. NO DEMOLITION DEBRIS OR EQUIPMENT WILL BE ALLOWED TO ENTER THE WATER AT ANY TIME. THE CONTRACTOR SHALL REMOVE ANY DEBRIS OR EQUIPMENT THAT ACCIDENTALLY ENTERS THE WATER AT HIS COST.
9. NO DEMOLITION DEBRIS WILL BE ALLOWED TO BE STOCKPILED WITHIN THE PROJECT LIMITS OR WITHIN 200' OF ANY SHORELINE FEATURE, AS DEFINED IN THE RHODE ISLAND COASTAL RESOURCES MANAGEMENT PROGRAM (RICRMP). ALL OFF-SITE STOCKPILES MUST BE IN ACCORDANCE WITH RIDOT STANDARD DRAINAGE AND EROSION CONTROL NOTES IN SHEET 3 OF THESE PLANS. DEBRIS SHALL BE LOADED DIRECTLY ONTO WAITING TRUCKS OR BARGES AND REMOVED FROM THE SITE REGULARLY FOR DISPOSAL AND/OR RECYCLING.
10. THE CONTRACTOR WILL NOT BE PERMITTED TO FURTHER BREAK UP THE REMOVED CONCRETE DECK PIECES BEYOND THAT NECESSARY FOR TRANSPORT OR TO SEPARATE THE REINFORCING STEEL FROM THE CONCRETE WITHIN THE PROJECT AREA - THE BRIDGE DECK DEBRIS WILL BE LOADED DIRECTLY ONTO WAITING TRUCKS OR BARGES AND REMOVED FROM THE SITE REGULARLY FOR DISPOSAL AND/OR RECYCLING.
11. THE CONTRACTOR SHALL PERFORM A POST-DEMOLITION SUBAQUEOUS INSPECTION OF THE NAVIGABLE CHANNEL AFTER ANY DEMOLITION ACTIVITIES THAT REQUIRE THE CLOSURE OF THE CHANNEL. THE SUBAQUEOUS INSPECTION SHALL BE PERFORMED PRIOR TO THE RE-OPENING OF THE CHANNEL AND SUBMITTED TO THE ENGINEER FOR THE U.S. COAST GUARD REVIEW AND APPROVAL. NAVIGABLE CHANNEL WILL NOT BE PERMITTED TO RE-OPEN WITHOUT U.S. COAST GUARD APPROVAL.

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**RHODE ISLAND
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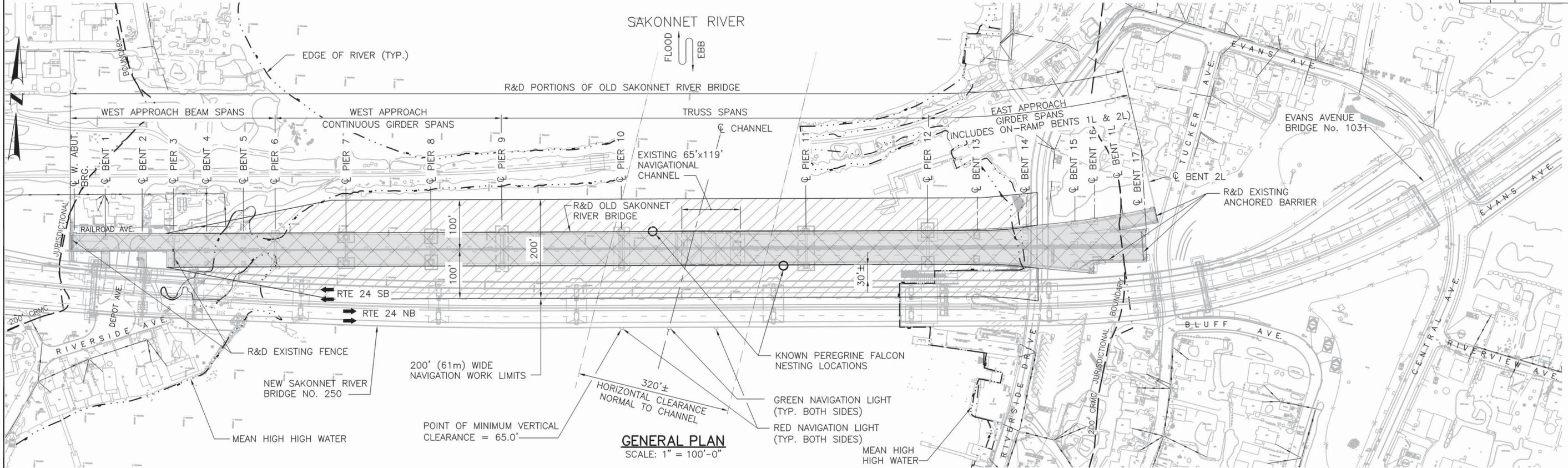
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SCALE:	NONE				

**BRIDGE DEMOLITION
OLD SAKONNET RIVER BRIDGE NO. 250**

PORTSMOUTH / TIVERTON RHODE ISLAND

DEMOLITION NOTES & DETAILS

SHEET DESIGNATION
G-8



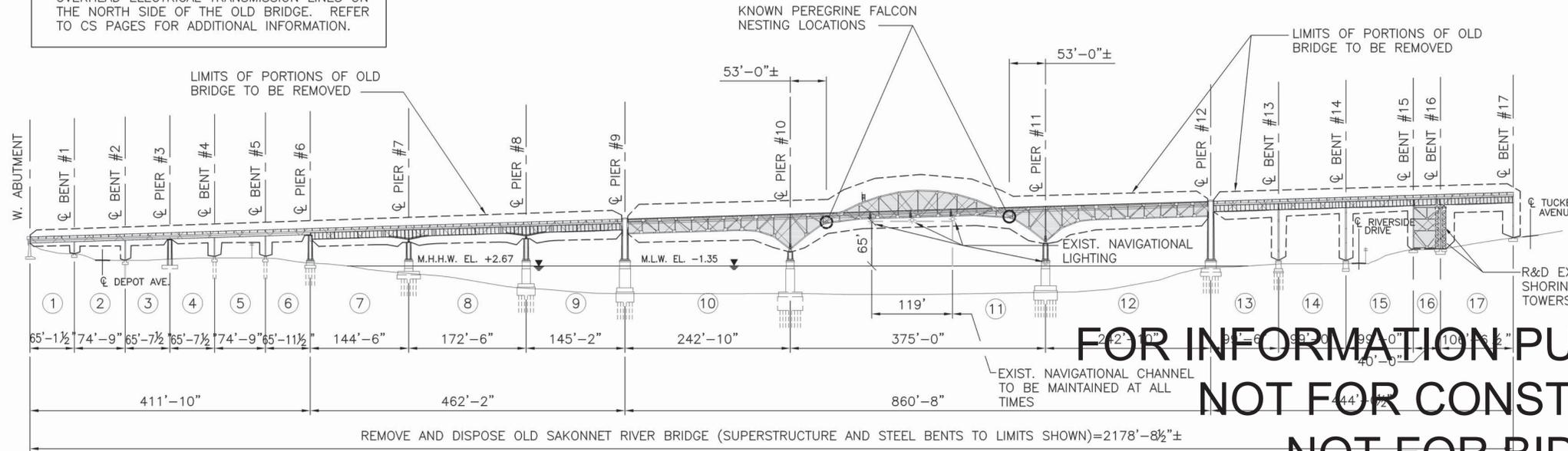
SPECIAL NOTE:
THE CONTRACTOR SHALL BE AWARE OF THE PRESENCE OF EXISTING HIGH VOLTAGE OVERHEAD ELECTRICAL TRANSMISSION LINES ON THE NORTH SIDE OF THE OLD BRIDGE. REFER TO CS PAGES FOR ADDITIONAL INFORMATION.

LEGEND

DENOTES AREAS TO BE REMOVED & DISPOSED (REFER TO "SUPERSTRUCTURE SECTIONS SHEETS 1-5, SUBSTRUCTURE ELEVATIONS SHEETS 1-3" AND BENT 16 TEMPORARY SHORING PLAN AND ELEVATION FOR DETAILED LIMITS OF REMOVAL)

(X) DENOTES SPAN NUMBER

- NOTES:**
1. THE LIMITS OF REMOVAL SHOWN ON THESE PLAN SHEETS DEPICTS THE PORTION OF THE OLD BRIDGE TO BE DEMOLISHED UNDER THIS CONTRACT. THE DEMOLITION SHALL BE PERFORMED IN ACCORDANCE WITH THE MAINTENANCE AND PROTECTION OF TRAFFIC PLANS INCLUDED IN THESE PLANS AND THE TIMES AND RESTRICTIONS NOTED IN THE TMP AND CS PAGES.
 2. DETAILS SHOWN ARE FOR ILLUSTRATIVE PURPOSES ONLY. THE CONTRACTOR IS RESPONSIBLE FOR REFERRING TO THE ORIGINAL CONSTRUCTION PLANS, SUBSEQUENT REPAIR CONTRACT PLANS, BRIDGE REPLACEMENT PLANS, AS WELL AS FIELD VERIFYING THE EXISTING CONFIGURATION OF THE BRIDGE TO DETERMINE THE COMPONENTS TO BE REMOVED.
 3. ALL UPLAND AREAS DISTURBED BY THE CONTRACTOR DUE TO HIS MEANS AND METHODS DURING THE DEMOLITION AND REMOVAL WORK SHALL BE BACKFILLED WITH COMMON BORROW, TOP-DRESSED WITH 4" PLANTABLE SOIL AND SEEDED.
 4. NO WORK OR PLACEMENT OF EQUIPMENT AND/OR TEMPORARY SUPPORTS IS PERMITTED PRIOR TO UTILIZATION OF THE ENGINEER, IN ACCORDANCE WITH NOTE 1 OF THE TEMPORARY CONSTRUCTION ACCESS NOTES" ON SHEET 7 OF THESE PLANS AND NOTE 3 OF THE "BRIDGE DEMOLITION NOTES" ON SHEET 9 OF THESE PLANS. NO BRIDGE DEMOLITION ACTIVITY BELOW M.H.H.W. IS PERMITTED UNDER THIS CONTRACT.
 5. ALL ELEVATIONS ARE IN FEET AND REFERENCE MEAN SEA LEVEL (MSL).



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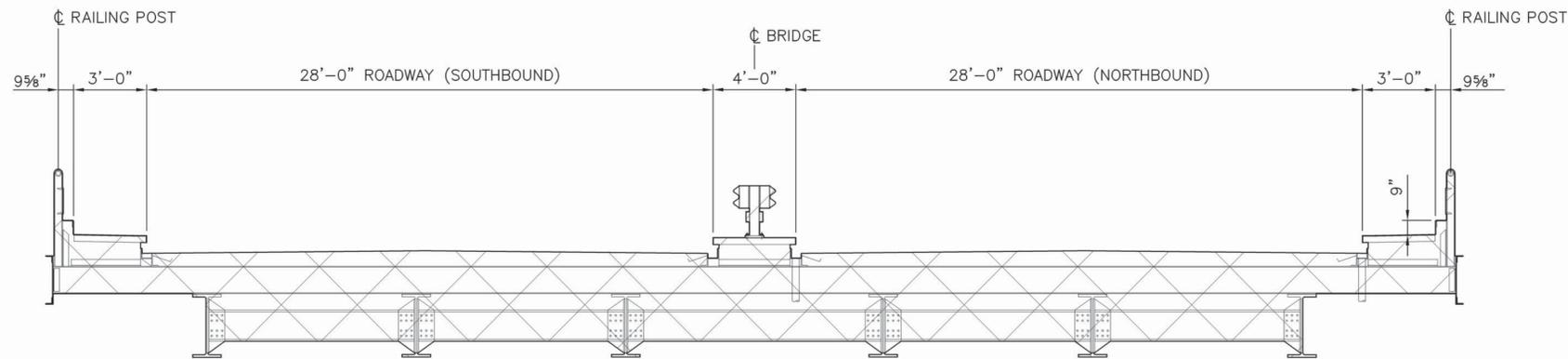
RHODE ISLAND
DEPARTMENT OF TRANSPORTATION

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DESIGNED BY:			DATE:		
CHECKED BY:			DATE:		
SCALE:	NONE				

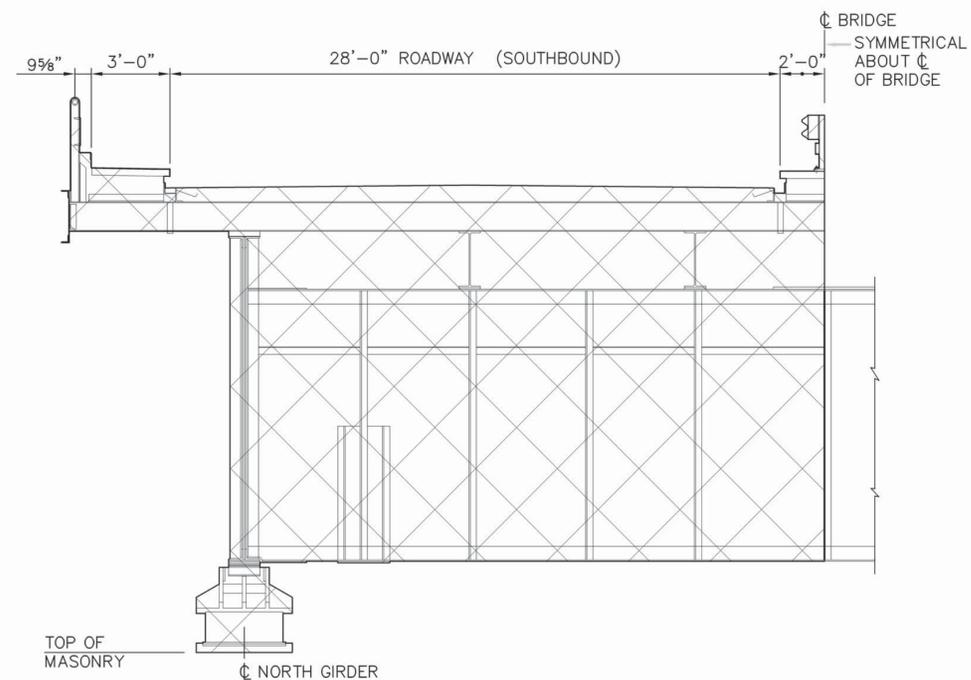
BRIDGE DEMOLITION
OLD SAKONNET RIVER BRIDGE NO. 250
PORTSMOUTH / TIVERTON
RHODE ISLAND

BRIDGE GENERAL PLAN & ELEVATION

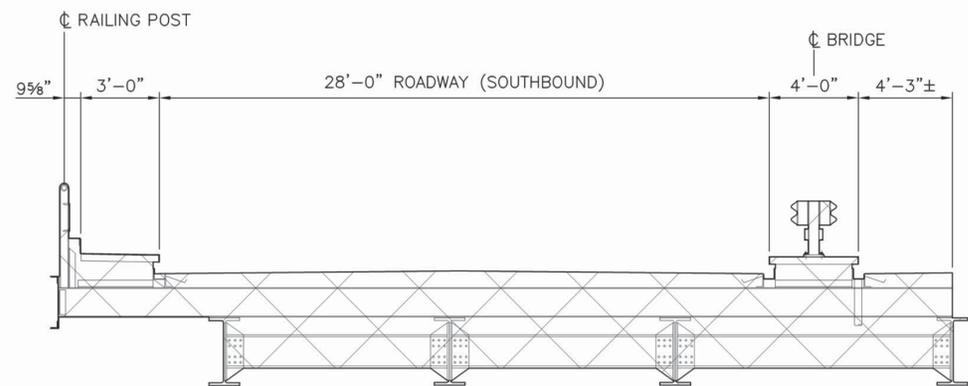
SHEET DESIGNATION
G-9



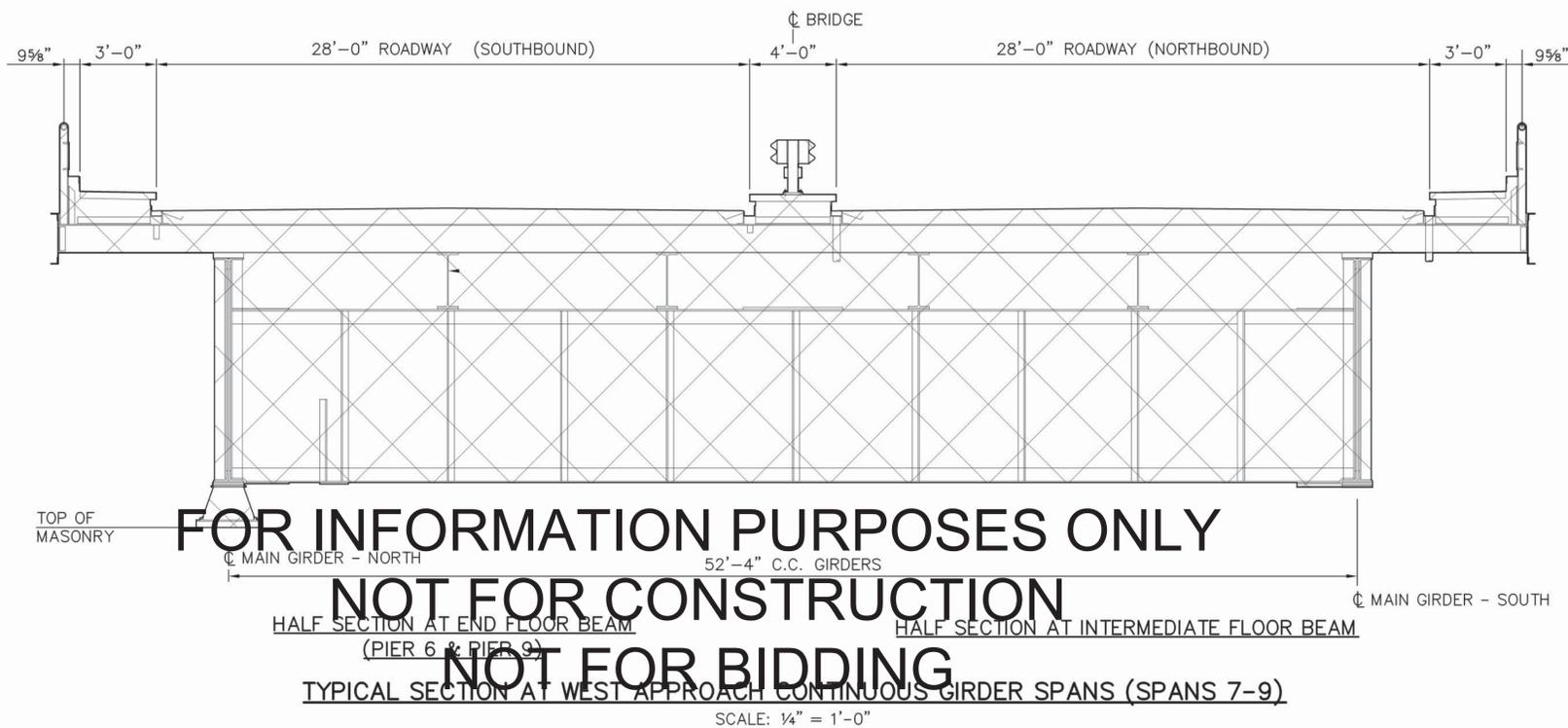
TYPICAL SECTION AT WEST APPROACH BEAM SPANS (SPANS 4-6)
SCALE: 1/4" = 1'-0"



HALF SECTION - FLOOR BEAM AT INTERMEDIATE SUPPORT (PIER 7 & PIER 8)
TYPICAL SECTION AT WEST APPROACH CONTINUOUS GIRDER SPANS (SPANS 7-9)
SCALE: 1/4" = 1'-0"



TYPICAL SECTION AT WEST APPROACH BEAM SPANS (SPANS 1-3)
SCALE: 1/4" = 1'-0"



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NOT FOR CONSTRUCTION
NOT FOR BIDDING

TYPICAL SECTION AT WEST APPROACH CONTINUOUS GIRDER SPANS (SPANS 7-9)
SCALE: 1/4" = 1'-0"

NOTE:

DETAILS SHOWN ARE FOR ILLUSTRATIVE PURPOSES ONLY. THE CONTRACTOR IS RESPONSIBLE FOR REFERRING TO THE ORIGINAL CONSTRUCTION PLANS, SUBSEQUENT REPAIR CONTRACT PLANS, BRIDGE REPLACEMENT PLANS, AS WELL AS FIELD VERIFYING THE EXISTING CONFIGURATION OF THE BRIDGE TO DETERMINE THE COMPONENTS TO BE REMOVED.

LEGEND



DENOTES AREAS TO BE REMOVED & DISPOSED

CEC# 03062.00



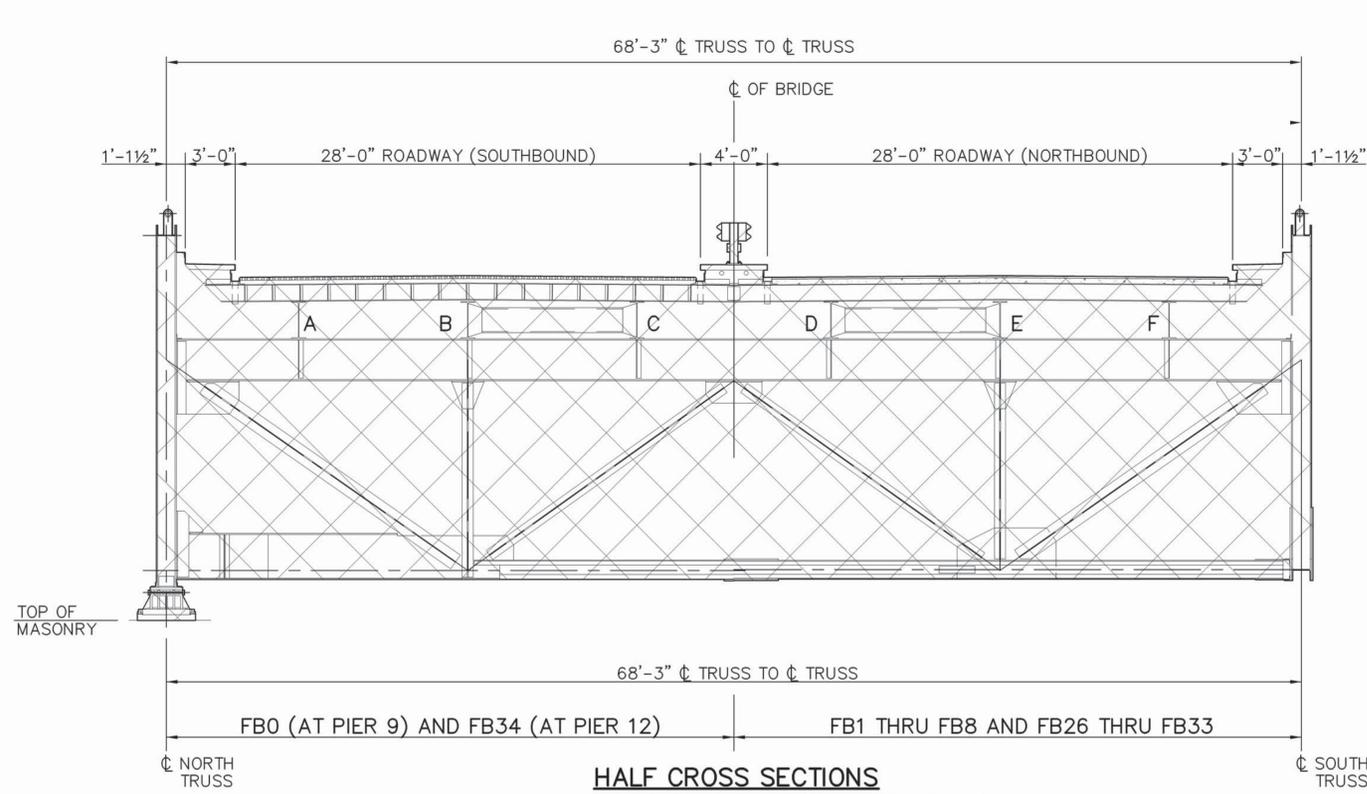
RHODE ISLAND
DEPARTMENT OF TRANSPORTATION

REVISIONS			REVISIONS		
NO.	DATE	BY	NO.	DATE	BY
DESIGNED BY:			DATE:		
CHECKED BY:			DATE:		
SCALE: AS SHOWN					

BRIDGE DEMOLITION
OLD SAKONNET RIVER BRIDGE NO. 250
PORTSMOUTH / TIVERTON
RHODE ISLAND

SUPERSTRUCTURE SECTIONS SHEET 1
SPANS 1 TO 9

SHEET DESIGNATION
D-1



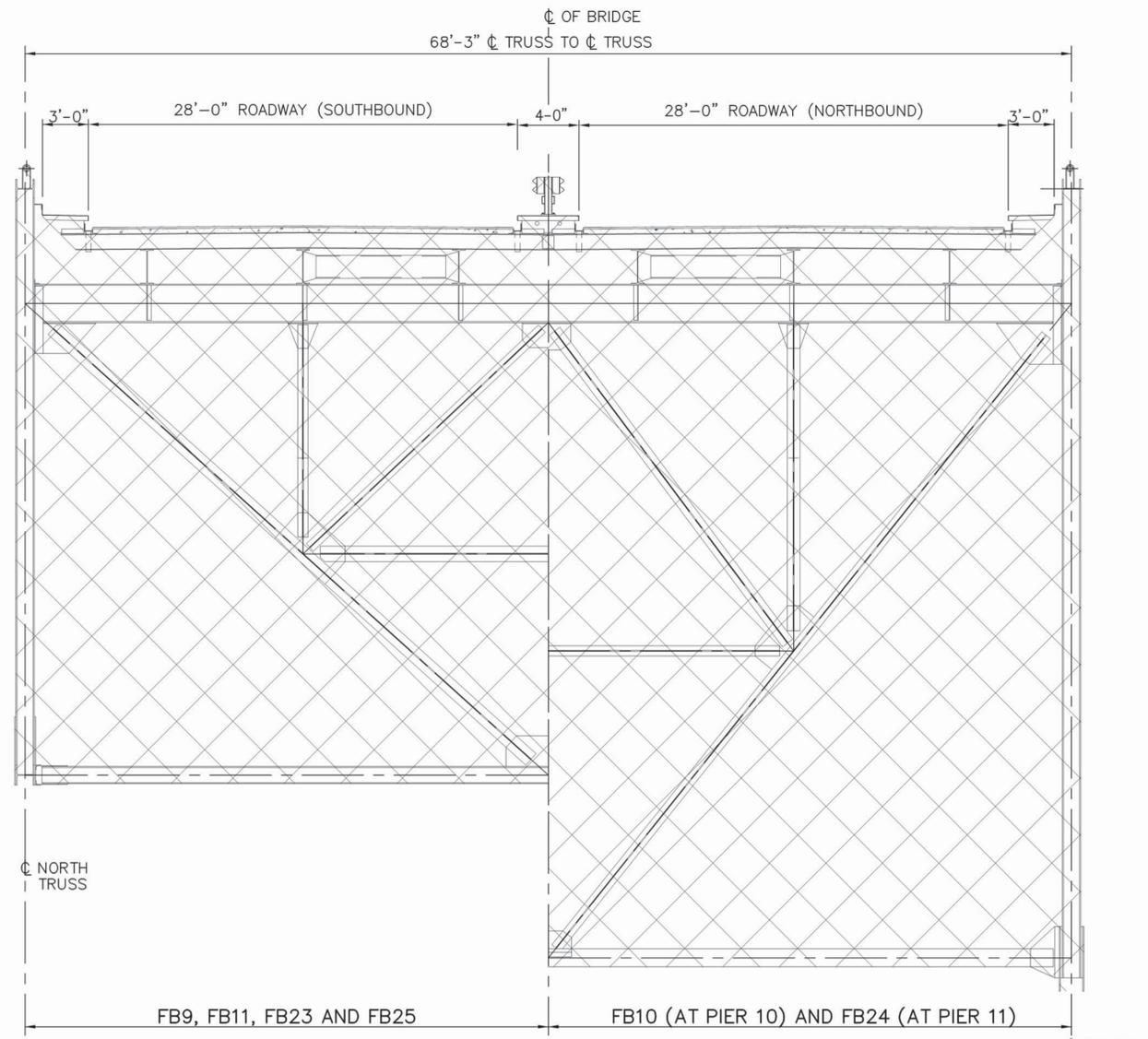
**HALF CROSS SECTIONS
AT TRUSS FLOOR BEAMS (SPANS 10-12)**
SCALE: 3/16" = 1'-0"

NOTE:

DETAILS SHOWN ARE FOR ILLUSTRATIVE PURPOSES ONLY. THE CONTRACTOR IS RESPONSIBLE FOR REFERRING TO THE ORIGINAL CONSTRUCTION PLANS, SUBSEQUENT REPAIR CONTRACT PLANS, BRIDGE REPLACEMENT PLANS, AS WELL AS FIELD VERIFYING THE EXISTING CONFIGURATION OF THE BRIDGE TO DETERMINE THE COMPONENTS TO BE REMOVED.

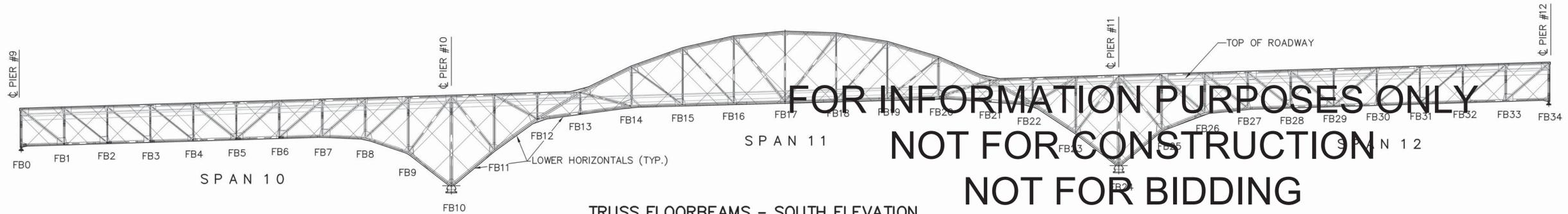
LEGEND

 DENOTES AREAS TO BE REMOVED & DISPOSED



HALF CROSS SECTIONS
SCALE: 3/16" = 1'-0"

TYPICAL SECTIONS AT TRUSS SPANS



TRUSS FLOORBEAMS - SOUTH ELEVATION
SCALE: 1/32" = 1'-0"

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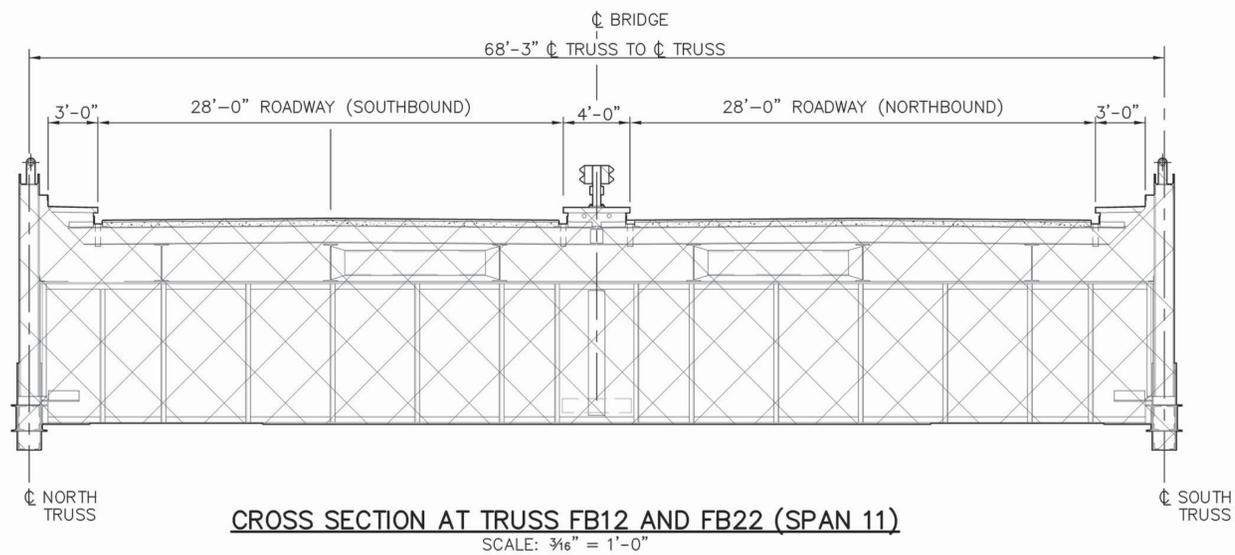
**RHODE ISLAND
DEPARTMENT OF TRANSPORTATION**

REVISIONS			REVISIONS		
NO.	DATE	BY	NO.	DATE	BY
DESIGNED BY:			DATE:		
CHECKED BY:			DATE:		
SCALE: AS SHOWN					

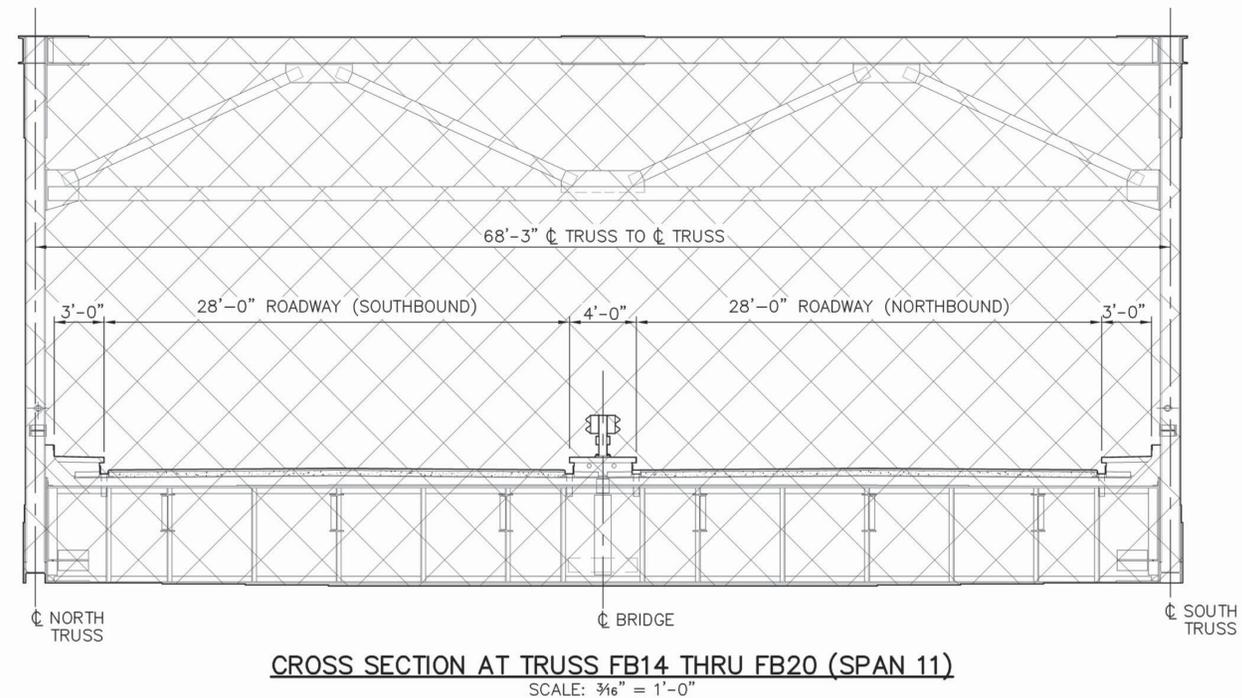
**BRIDGE DEMOLITION
OLD SAKONNET RIVER BRIDGE NO. 250**
PORTSMOUTH / TIVERTON
RHODE ISLAND

**SUPERSTRUCTURE SECTIONS SHEET 2
SPANS 10 TO 12**

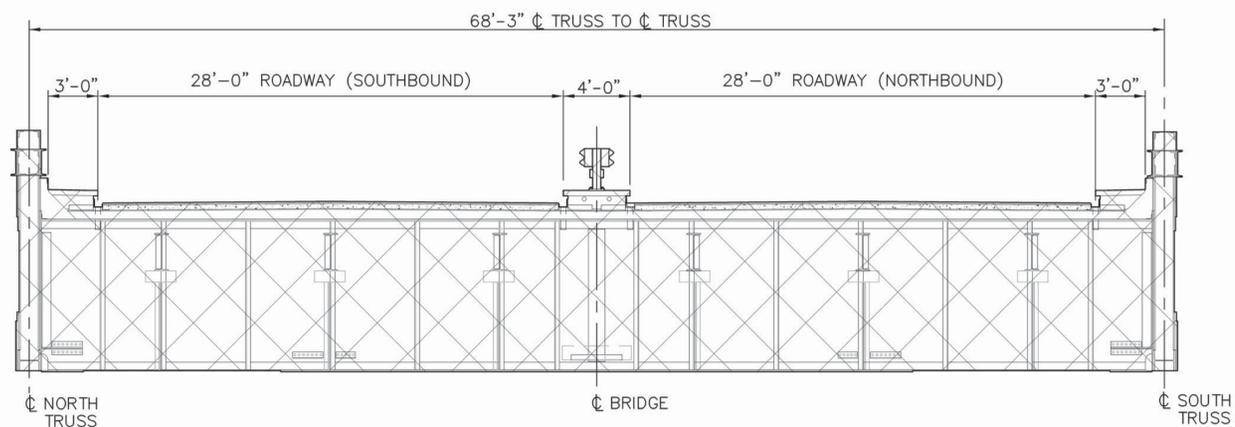
SHEET DESIGNATION
D-2



CROSS SECTION AT TRUSS FB12 AND FB22 (SPAN 11)
SCALE: 3/16" = 1'-0"



CROSS SECTION AT TRUSS FB14 THRU FB20 (SPAN 11)
SCALE: 3/16" = 1'-0"



CROSS SECTION AT TRUSS FB13 AND FB21 (SPAN 11)
SCALE: 3/16" = 1'-0"

NOTE:

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LEGEND

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TYPICAL SECTIONS AT TRUSS SPAN 11

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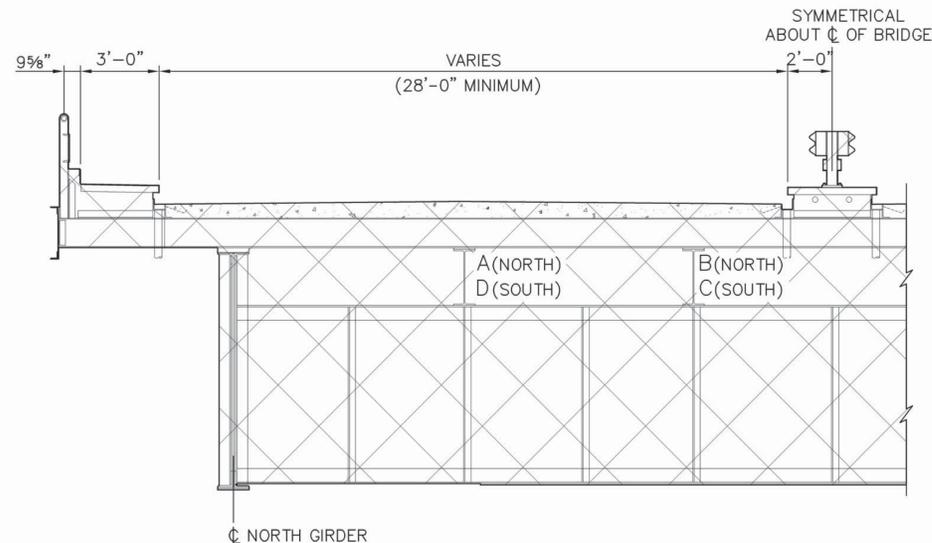
RHODE ISLAND
DEPARTMENT OF TRANSPORTATION

REVISIONS			REVISIONS		
NO.	DATE	BY	NO.	DATE	BY
DESIGNED BY:			DATE:		
CHECKED BY:			DATE:		
SCALE:	AS SHOWN				

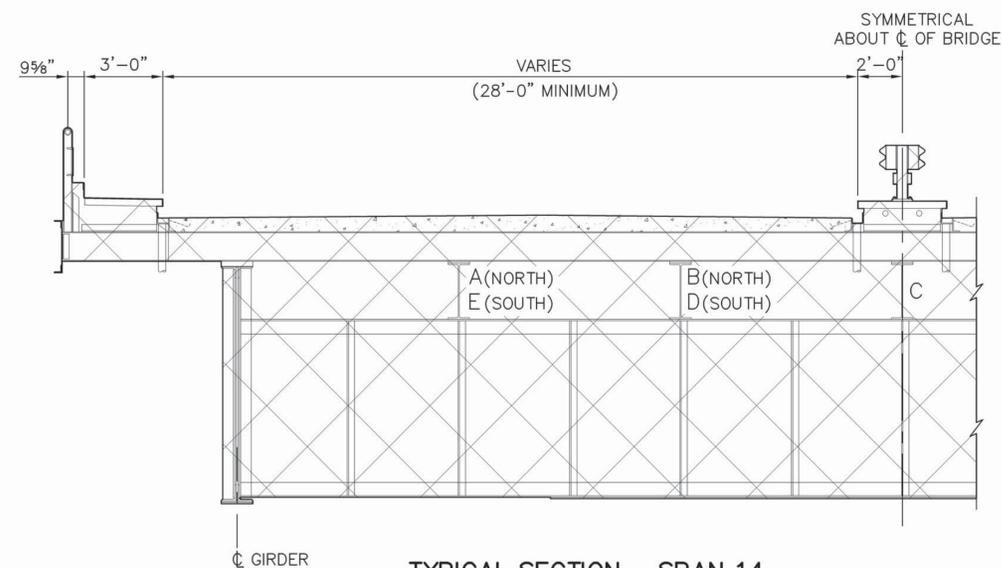
BRIDGE DEMOLITION
OLD SAKONNET RIVER BRIDGE NO. 250
PORTSMOUTH / TIVERTON
RHODE ISLAND

SUPERSTRUCTURE SECTIONS SHEET 3
SPAN 11

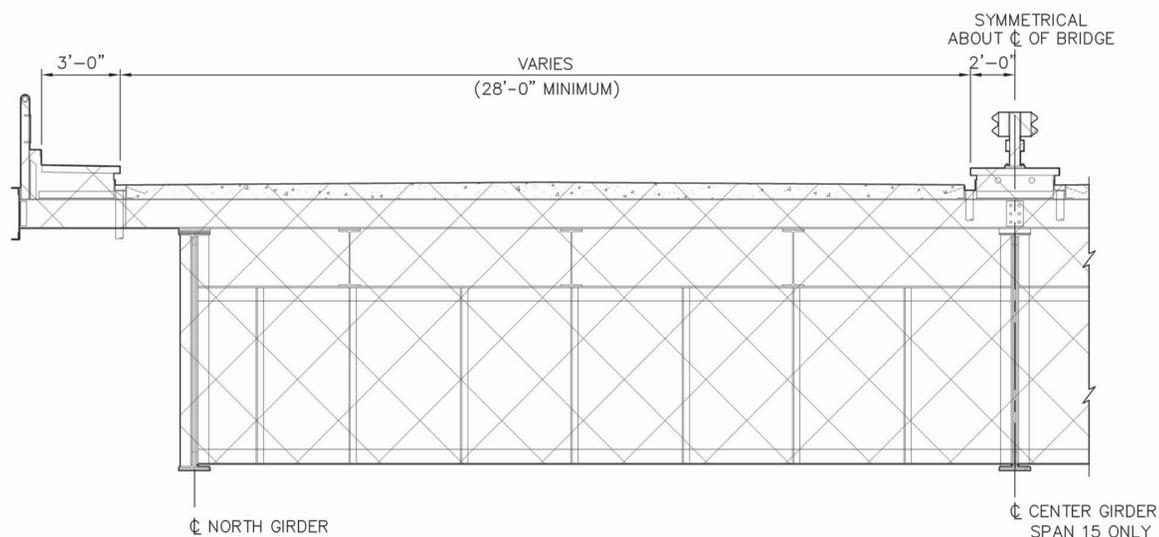
SHEET DESIGNATION
D-3



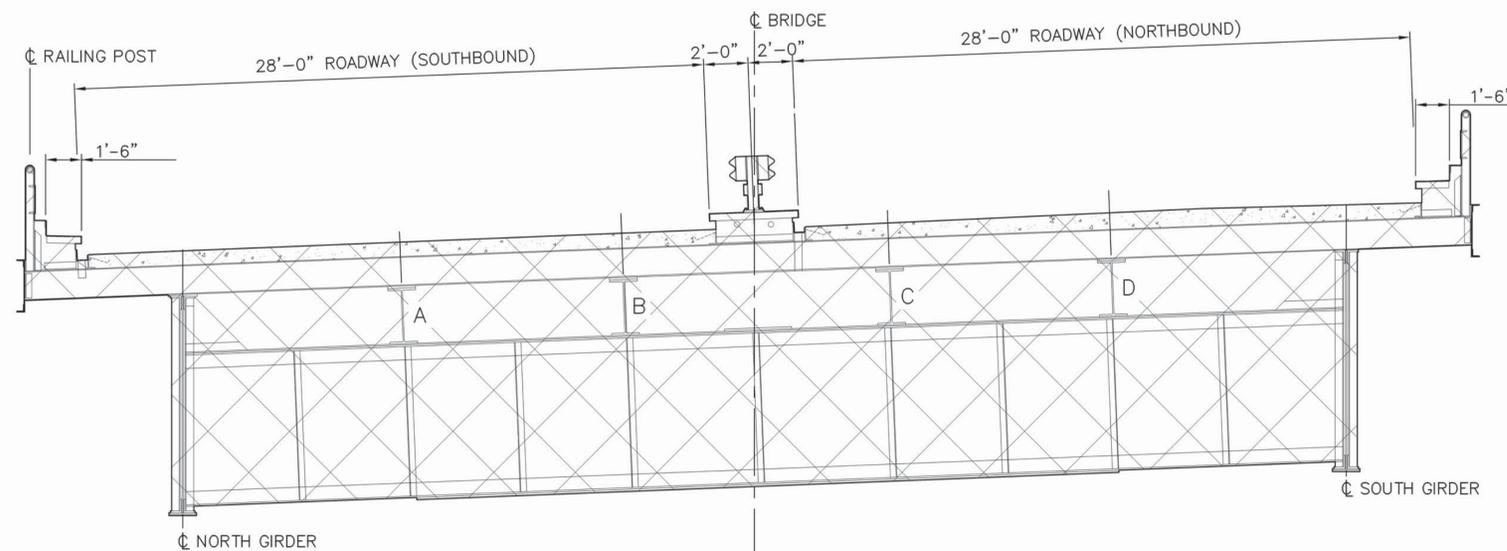
TYPICAL SECTION - SPAN 13
SCALE: 1/4" = 1'-0"



TYPICAL SECTION - SPAN 14
SCALE: 1/4" = 1'-0"



TYPICAL SECTION - SPAN 15
SCALE: 1/4" = 1'-0"
(FB 11, FB 12 SIMILAR WITH ADDL. STRINGERS)



TYPICAL SECTION - SPAN 16 & 17
SCALE: 1/4" = 1'-0"

NOTE:

DETAILS SHOWN ARE FOR ILLUSTRATIVE PURPOSES ONLY. THE CONTRACTOR IS RESPONSIBLE FOR REFERRING TO THE ORIGINAL CONSTRUCTION PLANS, THE EQUIPMENT REQUIREMENT PLANS, BRIDGE REPLACEMENT PLANS, AS WELL AS FIELD MEASUREMENTS OF THE EXISTING CONFIGURATION OF THE BRIDGE TO DETERMINE THE COMPONENTS TO BE REMOVED.

FOR INFORMATION PURPOSES ONLY

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NOT FOR BIDDING



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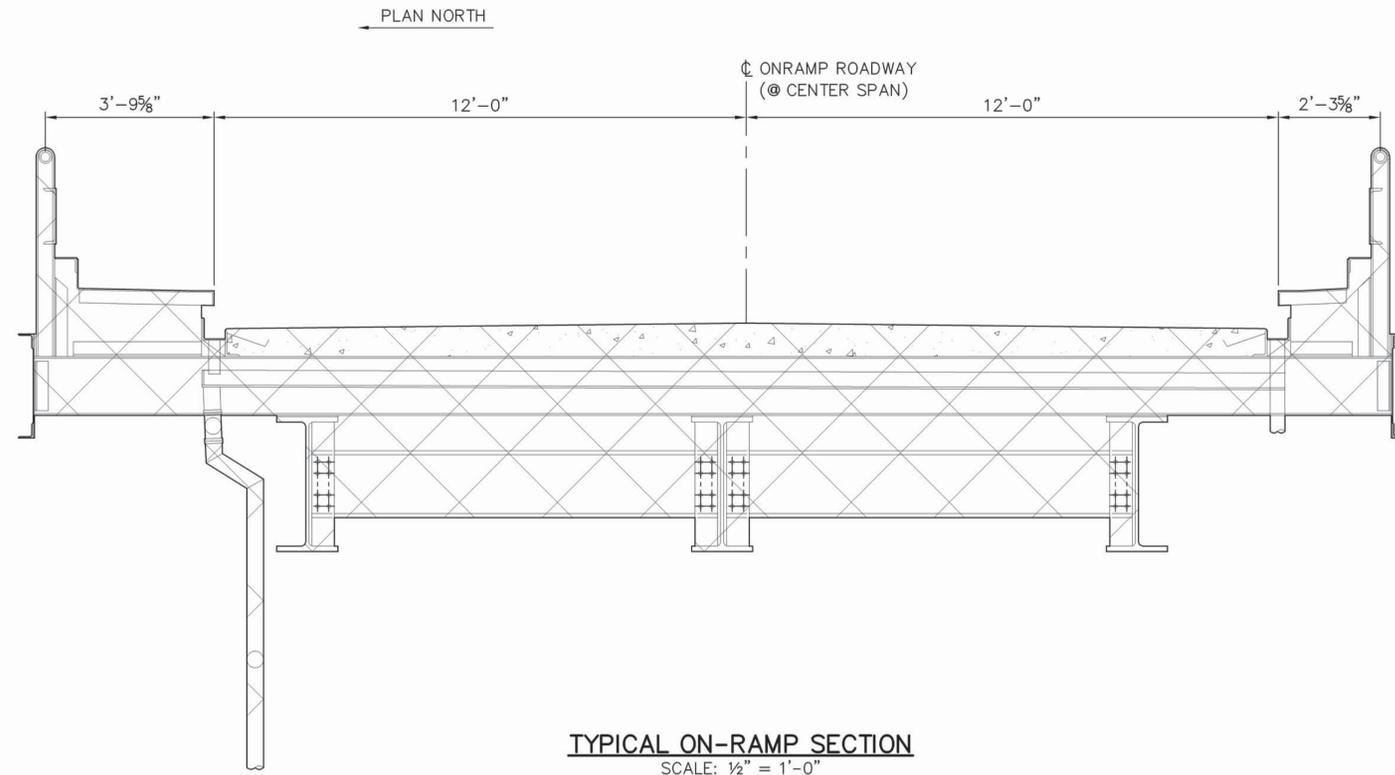
RHODE ISLAND
DEPARTMENT OF TRANSPORTATION

REVISIONS			REVISIONS		
NO.	DATE	BY	NO.	DATE	BY
DESIGNED BY:			DATE:		
CHECKED BY:			DATE:		
SCALE: AS SHOWN					

BRIDGE DEMOLITION
OLD SAKONNET RIVER BRIDGE NO. 250
PORTSMOUTH / TIVERTON
RHODE ISLAND

SUPERSTRUCTURE SECTIONS SHEET 4
SPANS 13 TO 17

SHEET DESIGNATION
D-4



TYPICAL ON-RAMP SECTION
 SCALE: 1/2" = 1'-0"
 (BETWEEN BENT 1L & BENT 2L)

NOTE:

DETAILS SHOWN ARE FOR ILLUSTRATIVE PURPOSES ONLY. THE CONTRACTOR IS RESPONSIBLE FOR REFERRING TO THE ORIGINAL CONSTRUCTION FILES SUBMITTED WITH THE AIA CONTRACT PLANS, BRIDGE REPLACEMENT PLANS, AS WELL AS FIELD VERIFYING THE EXISTING CONFIGURATION OF THE BRIDGE TO DETERMINE THE COMPONENTS TO BE REMOVED.

FOR INFORMATION PURPOSES ONLY
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NOT FOR BIDDING



DENOTES AREAS TO BE REMOVED & DISPOSED

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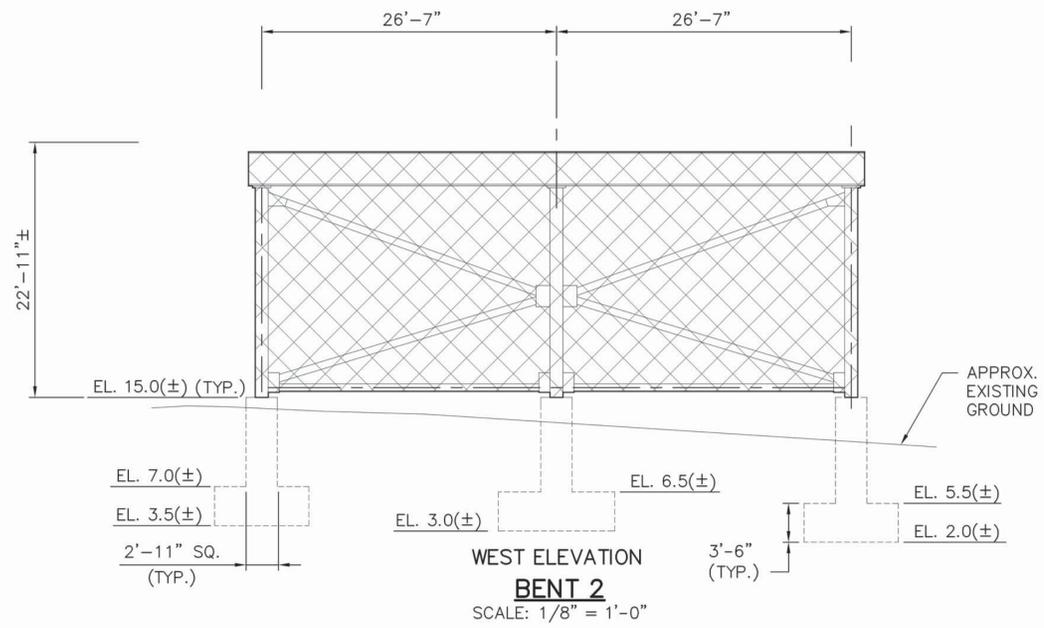
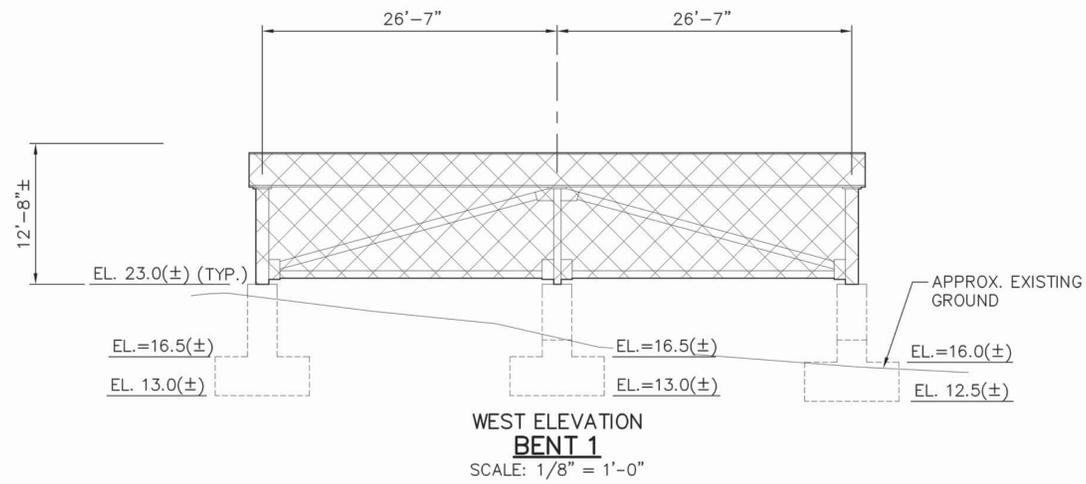
RHODE ISLAND
 DEPARTMENT OF TRANSPORTATION

REVISIONS			REVISIONS		
NO.	DATE	BY	NO.	DATE	BY
DESIGNED BY:		DATE:			
CHECKED BY:		DATE:			
SCALE:	AS SHOWN				

BRIDGE DEMOLITION
OLD SAKONNET RIVER BRIDGE NO. 250
 PORTSMOUTH / TIVERTON RHODE ISLAND

SUPERSTRUCTURE SECTIONS SHEET 5
TYPICAL ON RAMP

SHEET DESIGNATION
D-5



NOTES:

1. ALL ELEVATIONS ARE IN FEET AND REFERENCE MEAN SEA LEVEL (MSL). NATIONAL GEODETIC VERTICAL DATUM OF 1929 (NGVD29).
2. DETAILS SHOWN ARE FOR ILLUSTRATIVE PURPOSES ONLY. THE CONTRACTOR IS REFERRED TO THE ORIGINAL 1954 CONSTRUCTION PLANS FOR DIMENSIONS AND COMPOSITION OF SUBSTRUCTURE UNITS.
3. SUPERSTRUCTURE NOT SHOWN FOR CLARITY.

LEGEND

REMOVES AREAS TO BE REMOVED & DISPOSED

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NOT FOR BIDDING**

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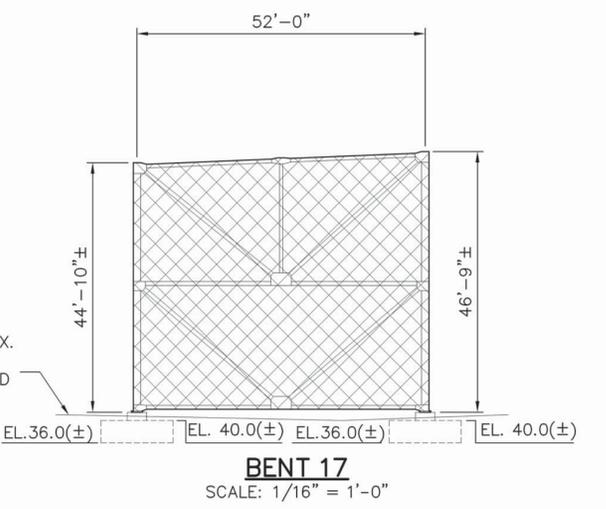
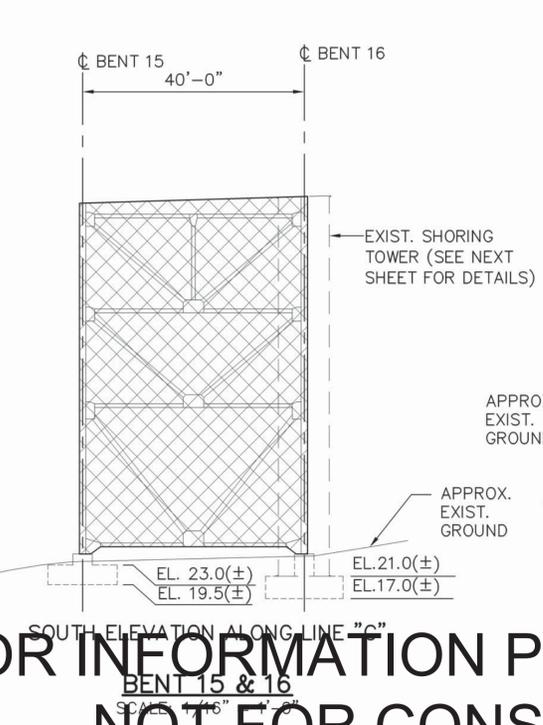
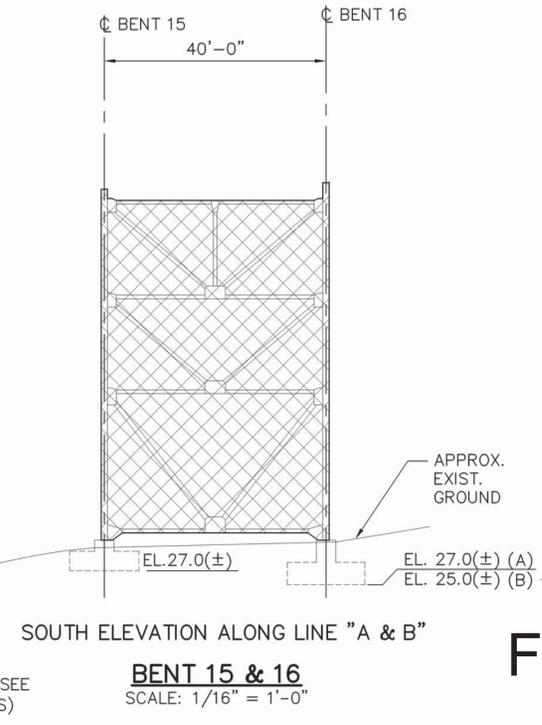
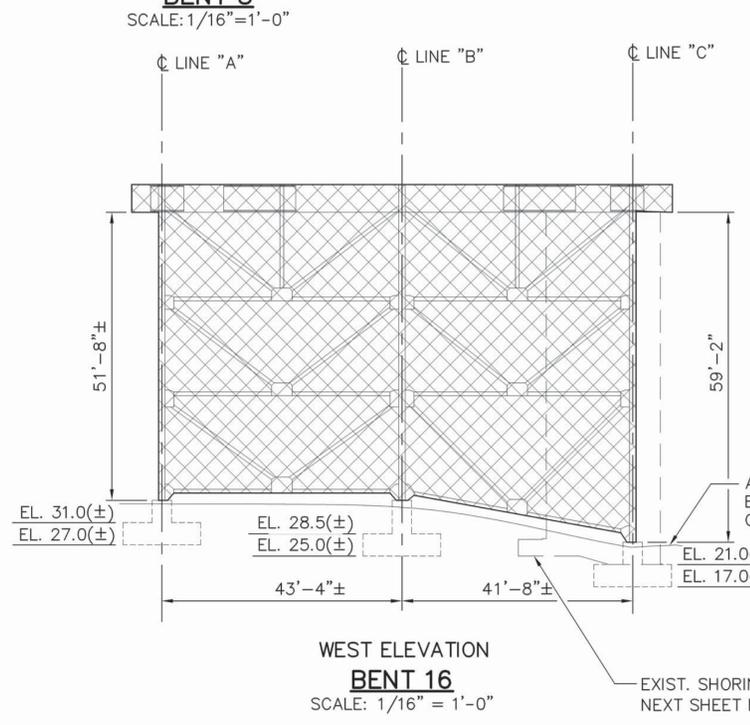
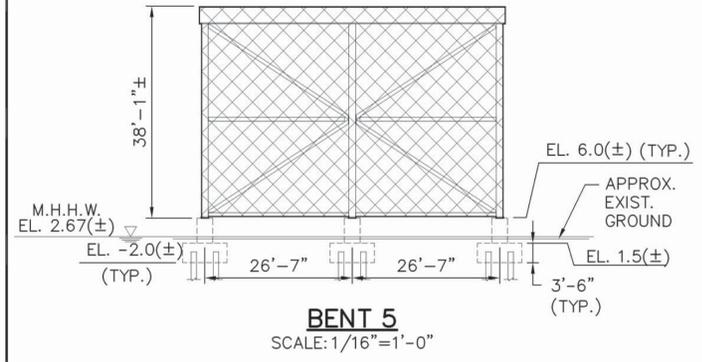
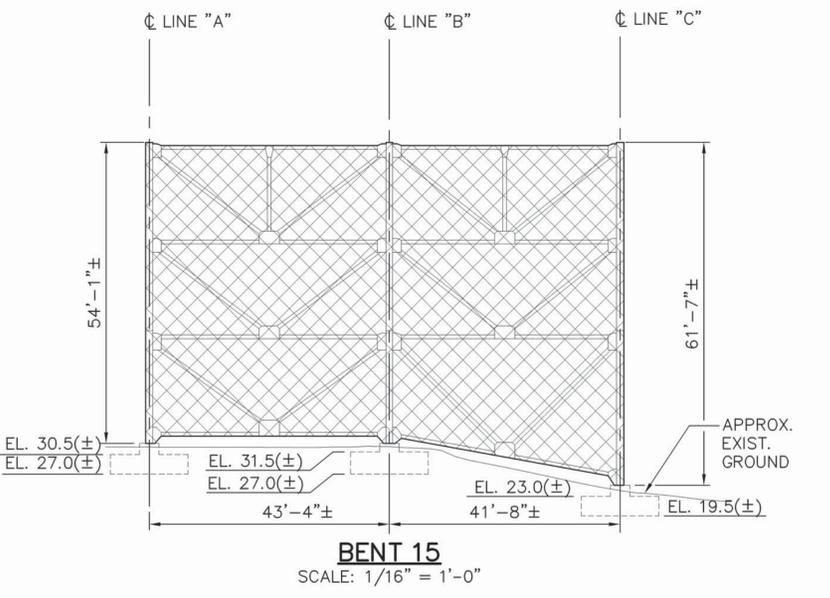
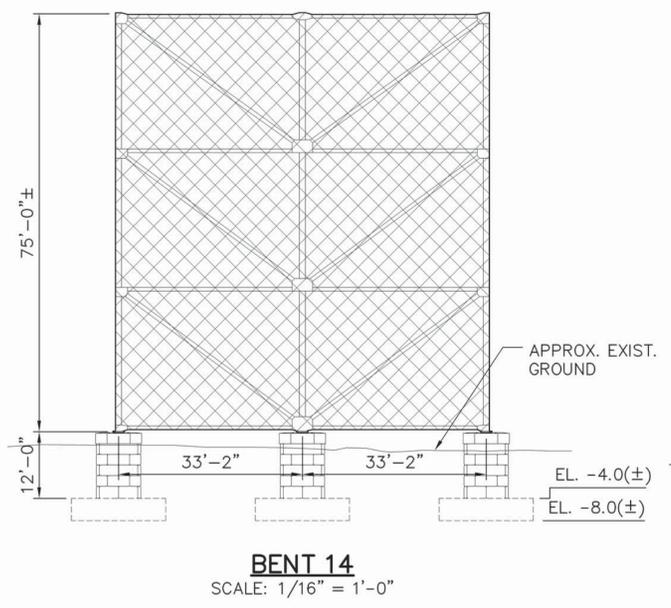
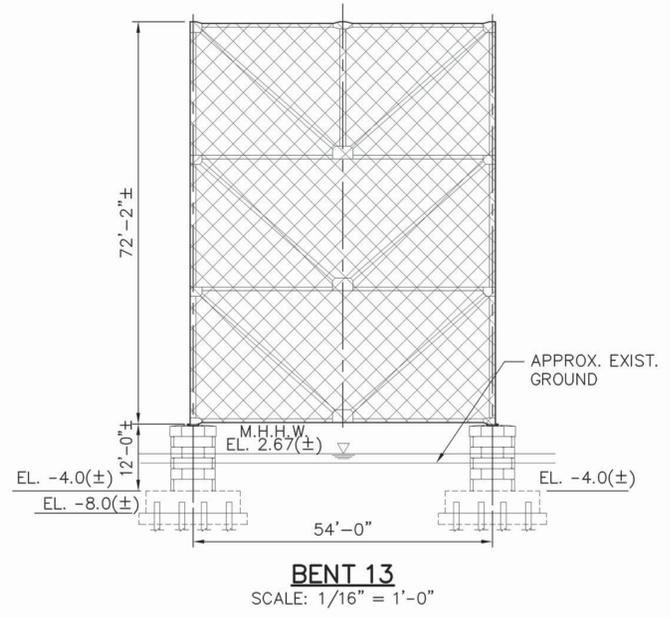
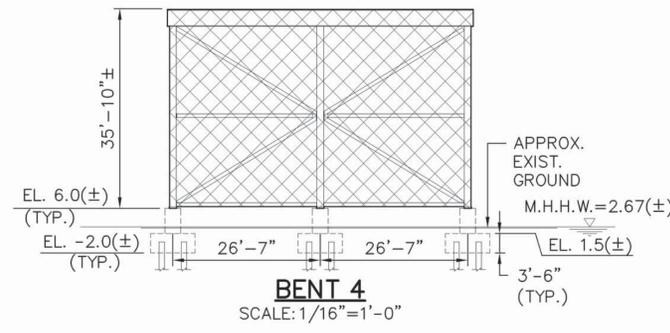
**RHODE ISLAND
DEPARTMENT OF TRANSPORTATION**

REVISIONS			REVISIONS		
NO.	DATE	BY	NO.	DATE	BY
DESIGNED BY:		DATE:			
CHECKED BY:		DATE:			
SCALE:	NONE				

**BRIDGE DEMOLITION
OLD SAKONNET RIVER BRIDGE NO. 250**
PORTSMOUTH / TIVERTON
RHODE ISLAND

**SUBSTRUCTURE ELEVATIONS SHEET 1
BENTS 1 & 2**

SHEET DESIGNATION
D-6



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NOT FOR BIDDING

- NOTES:**
1. ALL ELEVATIONS ARE IN FEET AND REFERENCE MEAN SEA LEVEL (MSL), NATIONAL GEODETIC VERTICAL DATUM OF 1929 (NGVD29).
 2. DETAILS SHOWN ARE FOR ILLUSTRATIVE PURPOSES ONLY. THE CONTRACTOR IS REFERRED TO THE ORIGINAL 1954 CONSTRUCTION PLANS FOR DIMENSIONS AND COMPOSITION OF SUBSTRUCTURE UNITS.
 3. SUPERSTRUCTURE NOT SHOWN FOR CLARITY.



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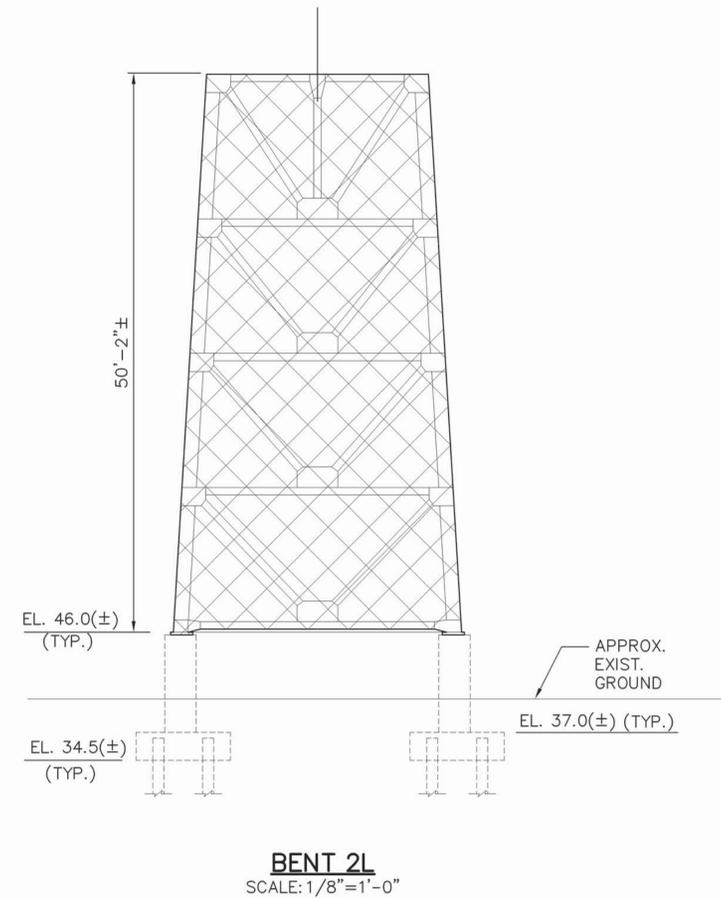
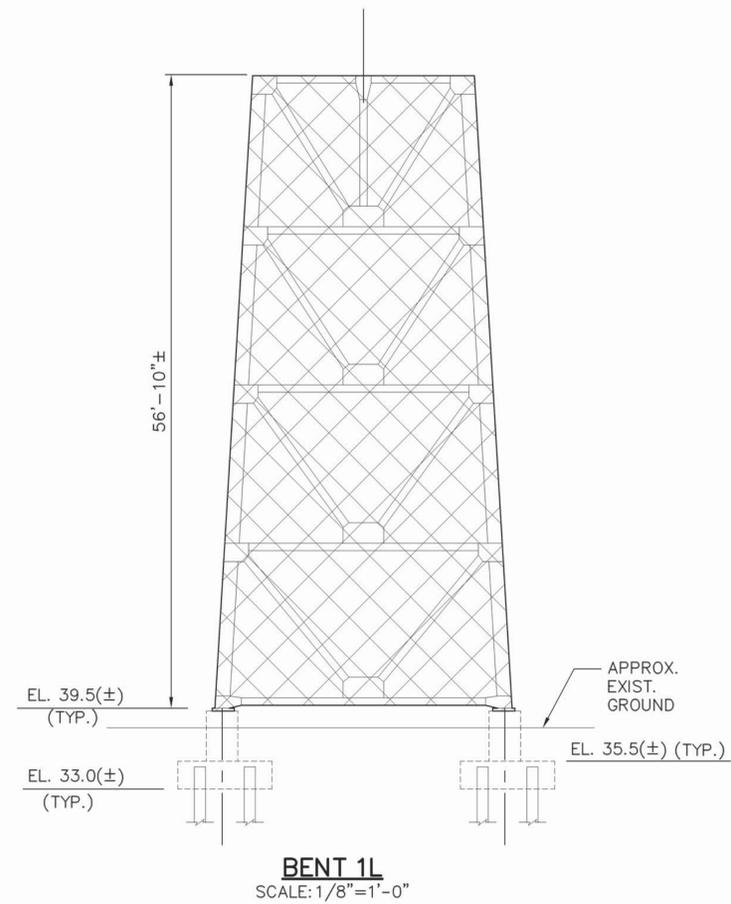
RHODE ISLAND DEPARTMENT OF TRANSPORTATION

REVISIONS			REVISIONS		
NO.	DATE	BY	NO.	DATE	BY
DESIGNED BY:			DATE:		
CHECKED BY:			DATE:		
SCALE: AS SHOWN					

BRIDGE DEMOLITION
OLD SAKONNET RIVER BRIDGE NO. 250
PORTSMOUTH / TIVERTON RHODE ISLAND

SUBSTRUCTURE ELEVATIONS SHEET 2
BENTS 4,5 & 13 TO 17

SHEET DESIGNATION: **D-7**



NOTES:

1. ALL ELEVATIONS ARE IN FEET AND REFERENCE MEAN SEA LEVEL (MSL). NATIONAL GEODETIC VERTICAL DATUM OF 1929 (NGVD29).
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3. SUPERSTRUCTURE NOT SHOWN FOR CLARITY.

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NOT FOR BIDDING**

LEGEND
 DENOTES AREAS TO BE REMOVED & DISPOSED

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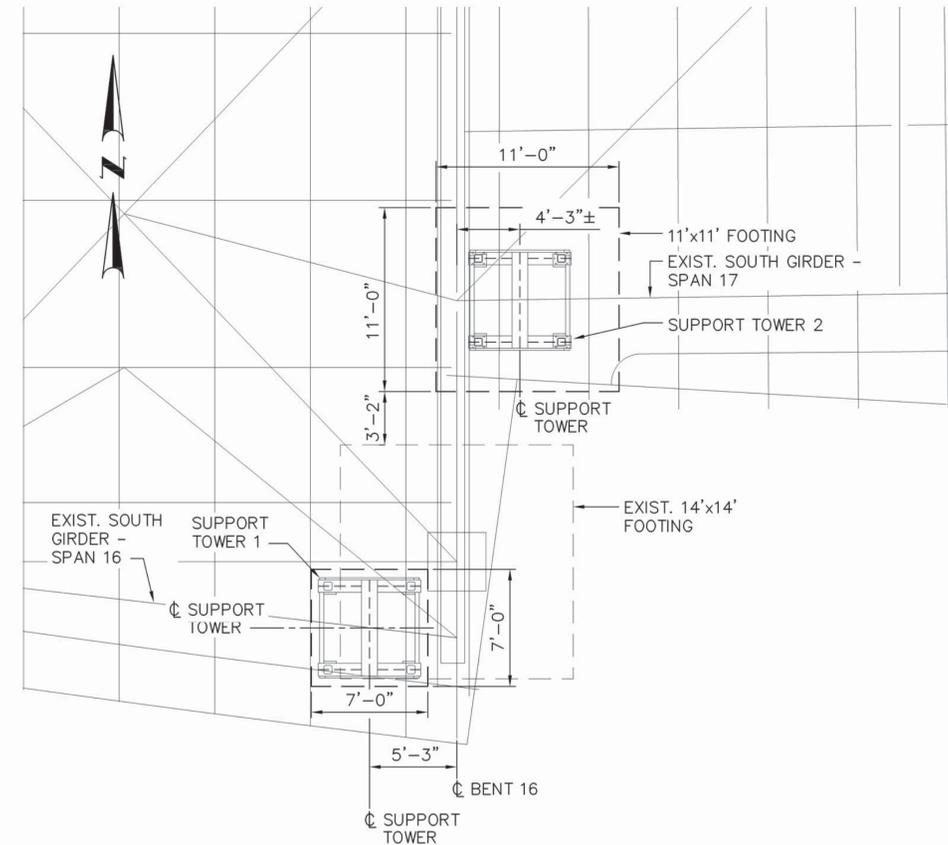
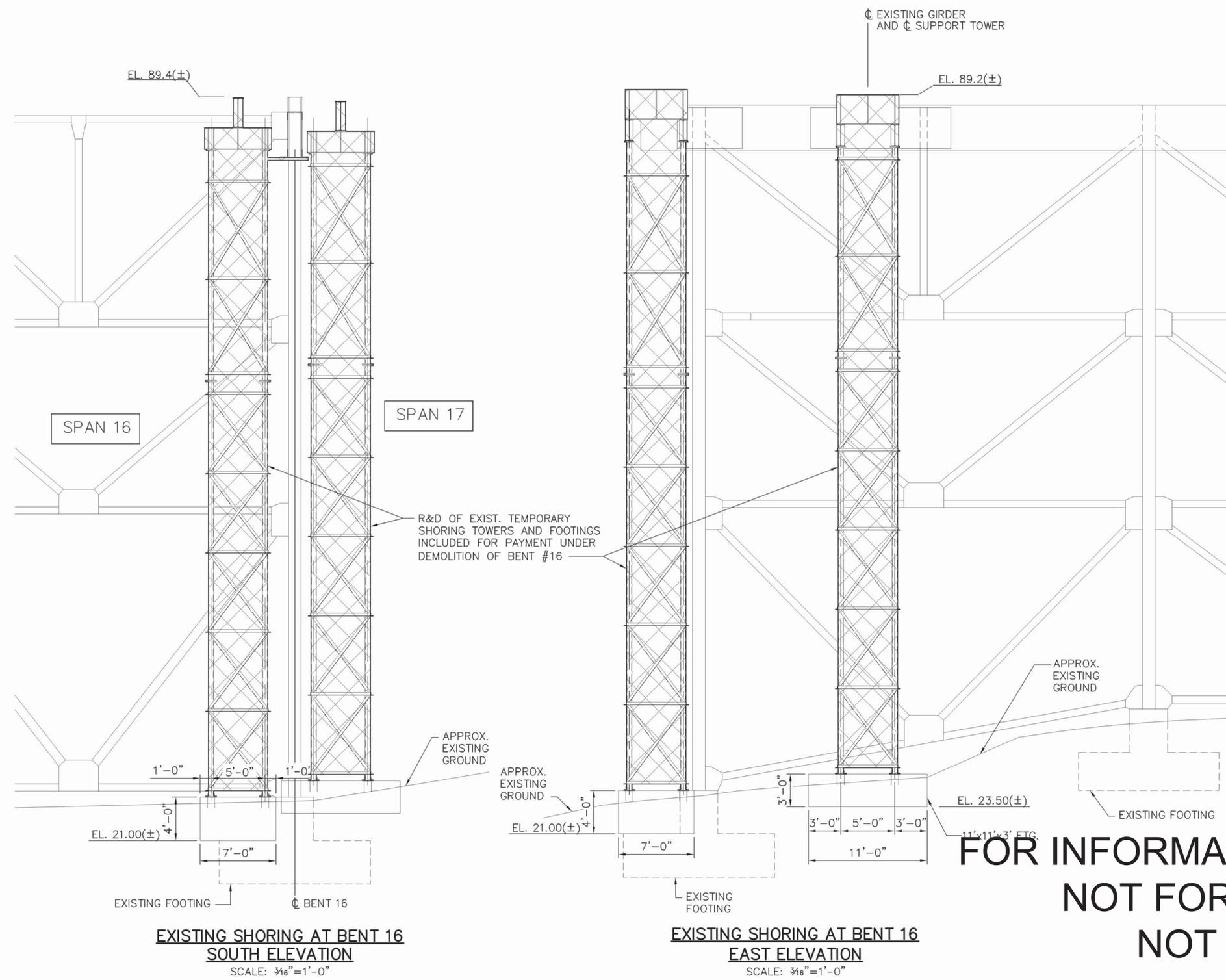
RHODE ISLAND
DEPARTMENT OF TRANSPORTATION

REVISIONS			REVISIONS		
NO.	DATE	BY	NO.	DATE	BY
DESIGNED BY:			DATE:		
CHECKED BY:			DATE:		
SCALE: AS SHOWN					

BRIDGE DEMOLITION
OLD SAKONNET RIVER BRIDGE NO. 250
PORTSMOUTH / TIVERTON RHODE ISLAND

SUBSTRUCTURE ELEVATIONS SHEET 4
BENTS 1L & 2L

SHEET DESIGNATION
D-8



PARTIAL PLAN VIEW
SCALE: 3/16"=1'-0"

NOTES:

- ALL ELEVATIONS ARE IN FEET AND REFERENCE MEAN SEA LEVEL (MSL). NATIONAL GEODETIC VERTICAL DATUM OF 1929 (NGVD29).
- DETAILS SHOWN ARE FOR ILLUSTRATIVE PURPOSES ONLY. THE CONTRACTOR IS REFERRED TO THE ORIGINAL 1954 CONSTRUCTION PLANS AND SUBSEQUENT INTERIM REPAIR CONTRACTS FOR DIMENSIONS AND COMPOSITION OF SUBSTRUCTURE UNITS AND SUPPLEMENTAL SHORING TOWERS.
- SUPERSTRUCTURE NOT SHOWN FOR CLARITY.
- BENT 16 SHOWN FOR ILLUSTRATIVE PURPOSE ONLY. REFER TO "SUBSTRUCTURE ELEVATIONS SHEET" FOR R&D LIMITS OF BENT 16.
- THE REMOVAL AND DISPOSAL OF THE TEMPORARY SHORING TOWERS SHOWN ON THIS SHEET SHALL BE INCLUDED FOR PAYMENT UNDER THE R&D OF BENT 16.

**FOR INFORMATION PURPOSES ONLY
NOT FOR CONSTRUCTION
NOT FOR BIDDING**

LEGEND

 DENOTES AREAS TO BE REMOVED & DISPOSED

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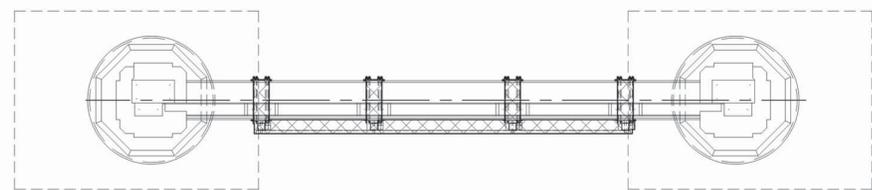
RHODE ISLAND
DEPARTMENT OF TRANSPORTATION

REVISIONS			REVISIONS		
NO.	DATE	BY	NO.	DATE	BY
DESIGNED BY:			DATE:		
CHECKED BY:			DATE:		
SCALE: AS SHOWN					

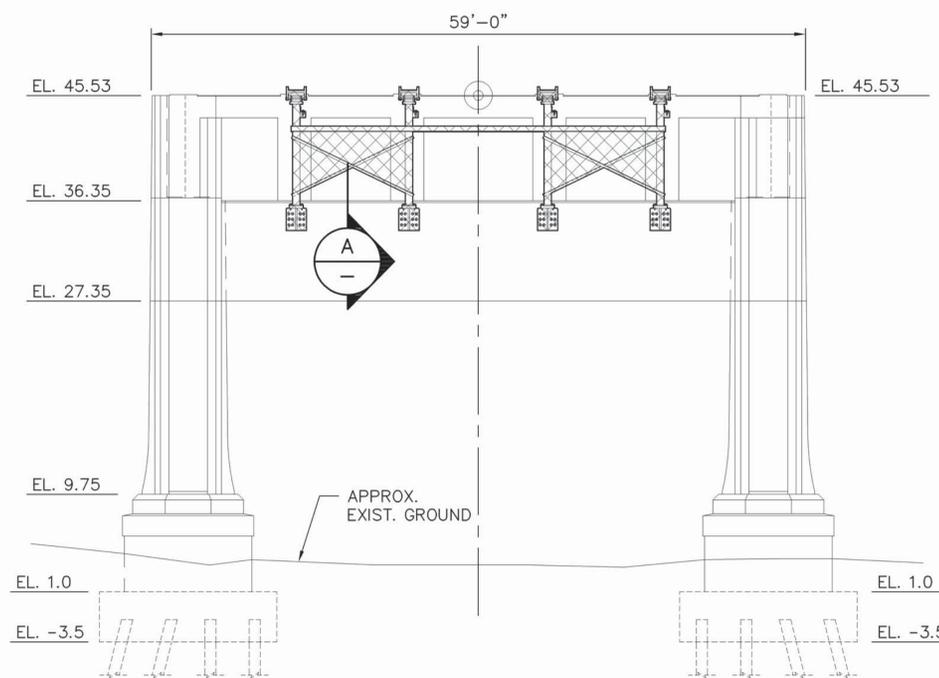
BRIDGE DEMOLITION
OLD SAKONNET RIVER BRIDGE NO. 250
PORTSMOUTH / TIVERTON
RHODE ISLAND

**BENT 16 TEMPORARY SHORING
PLAN AND ELEVATIONS**

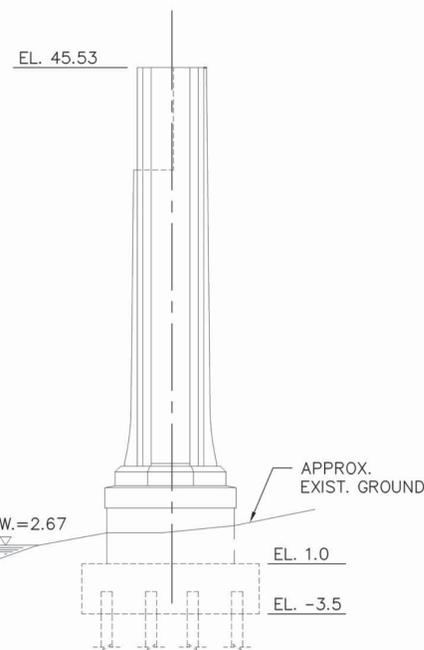
SHEET DESIGNATION
D-9



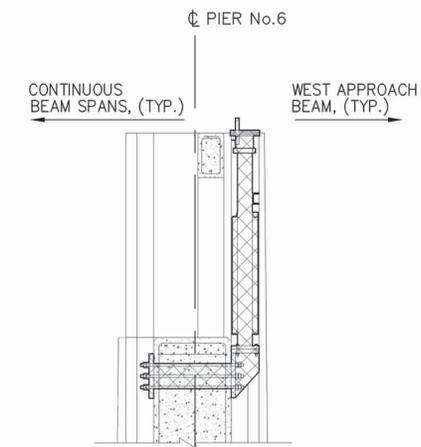
PLAN AT PIER No.6
SCALE: 1/8"=1'-0"



WEST ELEVATION
SCALE: 1/8"=1'-0"



NORTH ELEVATION
SCALE: 1/8"=1'-0"



SECTION A
SCALE: 1/4"=1'-0"

NOTES:

1. ALL ELEVATIONS ARE IN FEET AND REFERENCE MEAN SEA LEVEL (MSL), NATIONAL GEODETIC VERTICAL DATUM OF 1929 (NGVD29).
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3. SUPERSTRUCTURE NOT SHOWN FOR CLARITY.

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NOT FOR BIDDING**

LEGEND

DENOTES AREAS TO BE REMOVED & DISPOSED

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**RHODE ISLAND
DEPARTMENT OF TRANSPORTATION**

REVISIONS			REVISIONS		
NO.	DATE	BY	NO.	DATE	BY
DESIGNED BY:			DATE:		
CHECKED BY:			DATE:		
SCALE:	AS SHOWN				

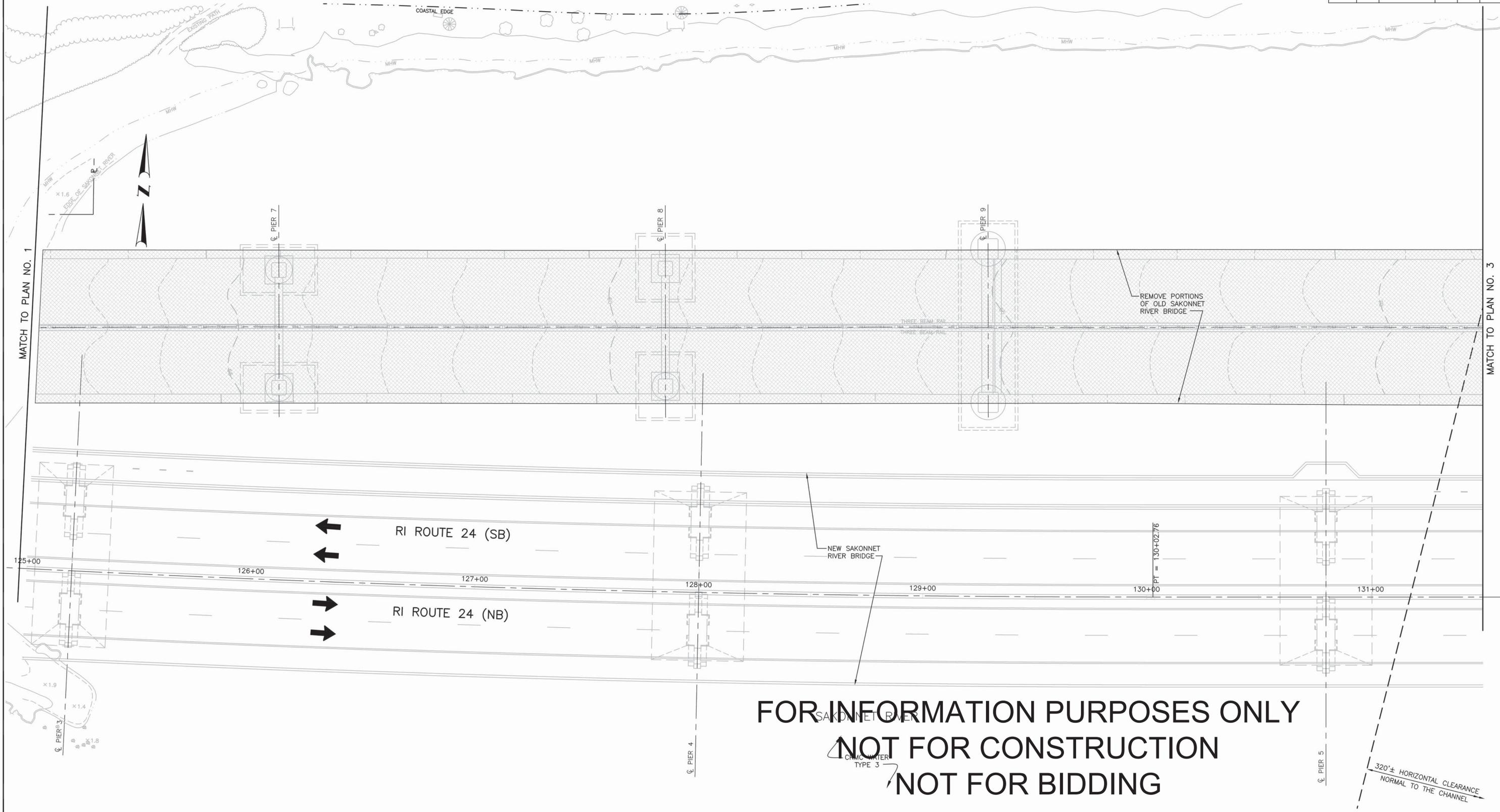
**BRIDGE DEMOLITION
OLD SAKONNET RIVER BRIDGE NO. 250**

PORTSMOUTH / TIVERTON
RHODE ISLAND

**PIER 6 TEMPORARY SHORING
PLAN AND ELEVATIONS**

SHEET DESIGNATION
D-10

FED. ROAD DIV. NO.	STATE	FEDERAL AID PROJECT NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
	RI			22	33



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400 SMITH STREET
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RHODE ISLAND
DEPARTMENT OF TRANSPORTATION

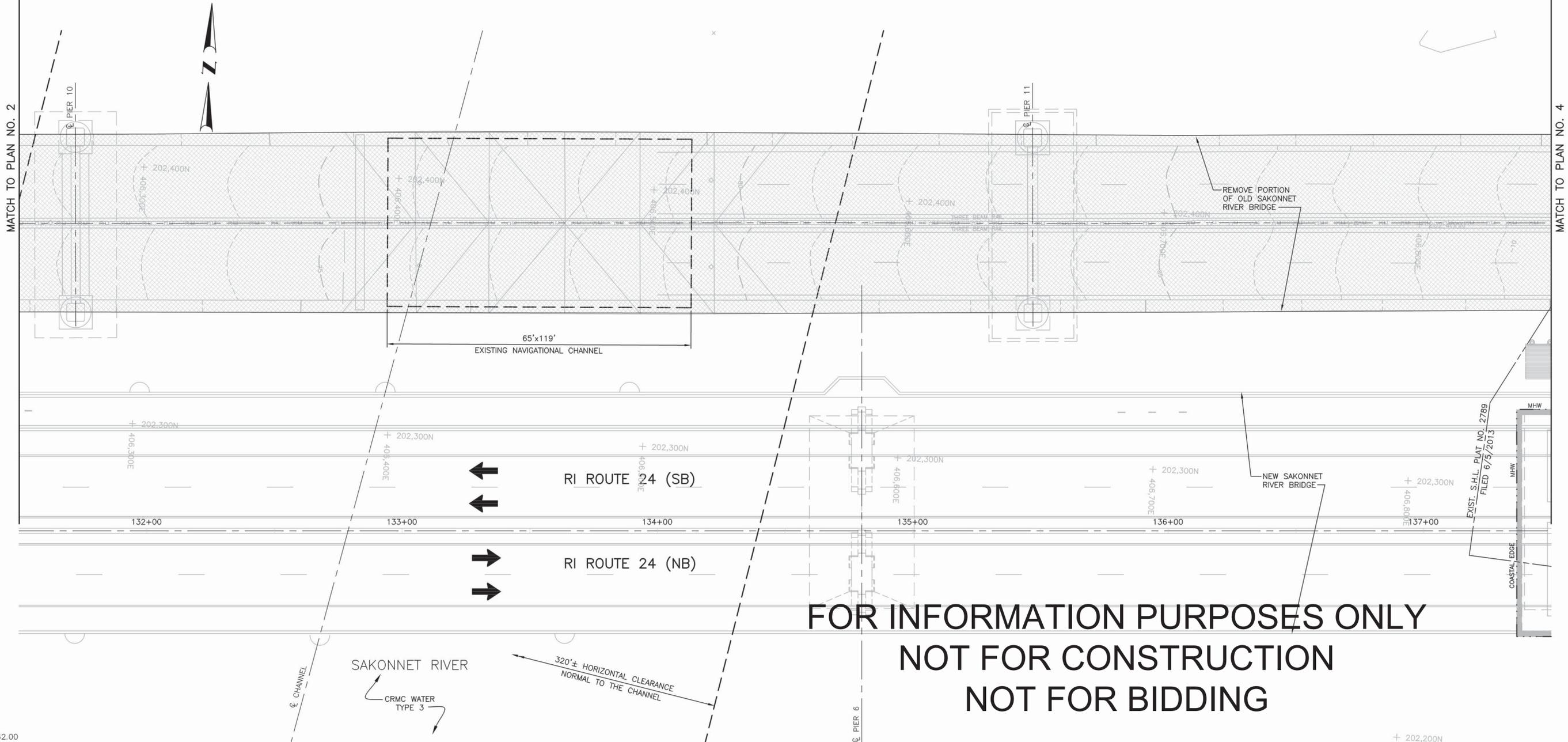
REVISIONS			REVISIONS		
NO.	DATE	BY	NO.	DATE	BY

DESIGNED BY: _____ DATE: _____
CHECKED BY: _____ DATE: _____
SCALE: 1"=20'

BRIDGE DEMOLITION
OLD SAKONNET RIVER BRIDGE NO. 250
PORTSMOUTH / TIVERTON
RHODE ISLAND

GENERAL PLAN NO. 2 - SPANS 7 TO 10

SHEET DESIGNATION
GP-2



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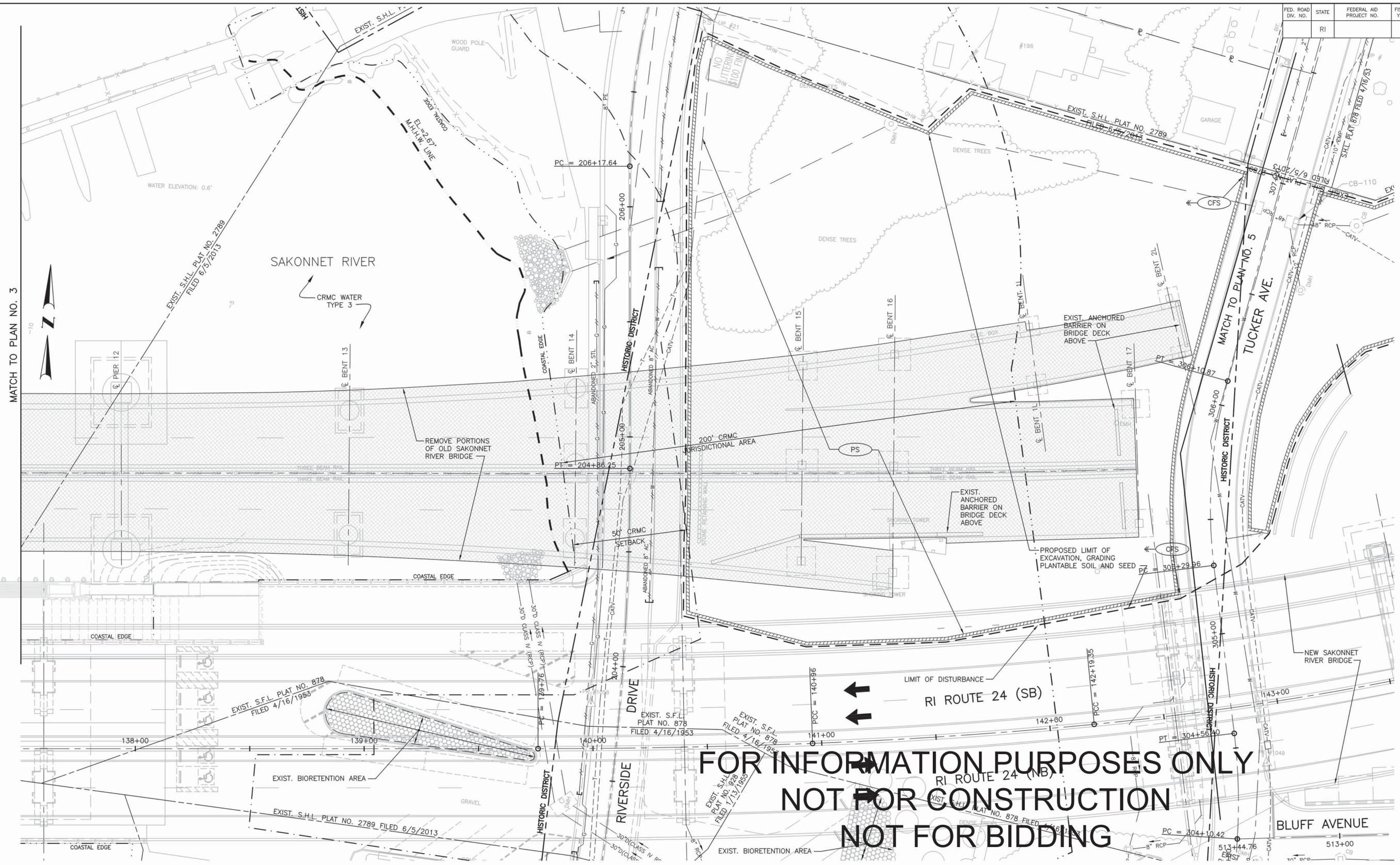
**RHODE ISLAND
DEPARTMENT OF TRANSPORTATION**

REVISIONS			REVISIONS		
NO.	DATE	BY	NO.	DATE	BY
DESIGNED BY:			DATE:		
CHECKED BY:			DATE:		
SCALE: 1"=20'					

**BRIDGE DEMOLITION
OLD SAKONNET RIVER BRIDGE NO. 250**
PORTSMOUTH / TIVERTON RHODE ISLAND

GENERAL PLAN NO. 3 - SPANS 10 & 11

SHEET DESIGNATION
GP-3



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DEPARTMENT OF TRANSPORTATION**

REVISIONS			REVISIONS		
NO.	DATE	BY	NO.	DATE	BY
DESIGNED BY:			DATE:		
CHECKED BY:			DATE:		
SCALE:	1"=20'				

**BRIDGE DEMOLITION
OLD SAKONNET RIVER BRIDGE NO. 250**
PORTSMOUTH / TIVERTON RHODE ISLAND

**GENERAL PLAN NO. 4
SPANS 11 TO 17, 1L, AND 2L**

SHEET DESIGNATION: **GP-4**



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PROVIDENCE, RI 02908



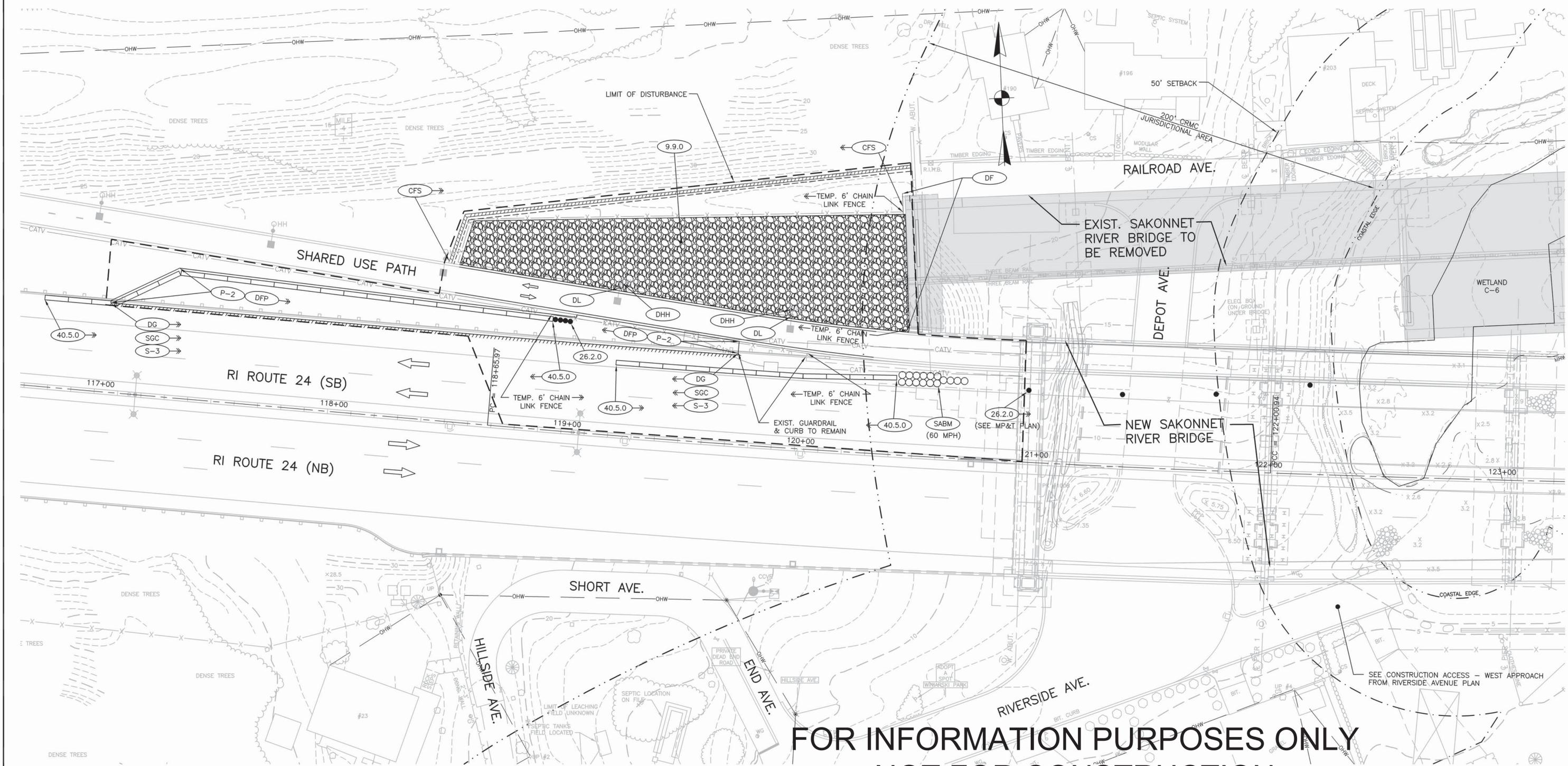
RHODE ISLAND
DEPARTMENT OF TRANSPORTATION

REVISIONS			REVISIONS		
NO.	DATE	BY	NO.	DATE	BY
DESIGNED BY:			DATE:		
CHECKED BY:			DATE:		
SCALE: 1"=20'					

BRIDGE DEMOLITION
OLD SAKONNET RIVER BRIDGE NO. 250
PORTSMOUTH / TIVERTON
RHODE ISLAND

GENERAL PLAN NO. 5
EAST APPROACH

SHEET DESIGNATION
GP-5



FOR INFORMATION PURPOSES ONLY
NOT FOR CONSTRUCTION
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CONSTRUCTION ACCESS ROAD
 SCALE: 1"=40'

1. CONTRACTOR SHALL RESTORE THE CONSTRUCTION ACCESS BACK TO THE ORIGINAL CONDITION (I.E. PAVEMENT FOR DRIVE USE WITH GRANULAR SOIL, GUARDRAIL, GRANITE CURB, ETC.) AT THE END OF THE CONTRACT.
2. CONTRACTOR SHALL RESTORE THE CONSTRUCTION ACCESS AREA WITH THE TEMPORARY 6' HIGH CHAIN LINK FENCE ON BOTH SIDE OF SHARED USE PATH AS SHOWN ON THE PLAN AT THE END OF THE WORKING DAY.

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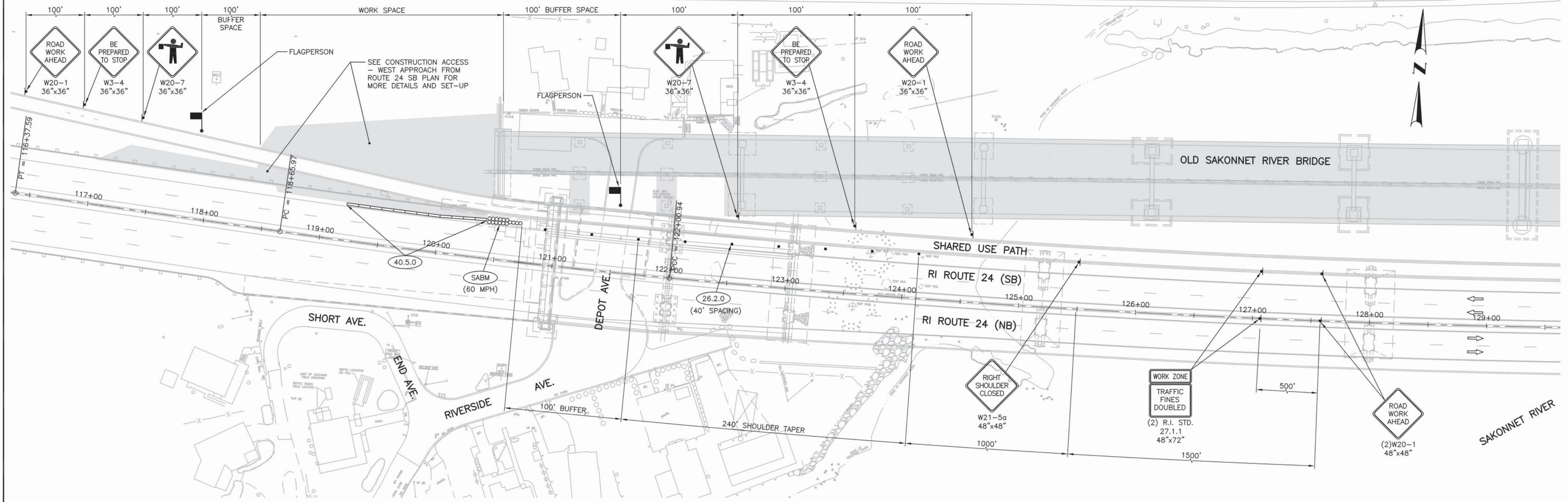
**RHODE ISLAND
 DEPARTMENT OF TRANSPORTATION**

REVISIONS			REVISIONS		
NO.	DATE	BY	NO.	DATE	BY
DESIGNED BY:			DATE:		
CHECKED BY:			DATE:		
SCALE:					

**BRIDGE DEMOLITION
 OLD SAKONNET RIVER BRIDGE NO. 250**
 PORTSMOUTH / TIVERTON RHODE ISLAND

**CONSTRUCTION ACCESS SHEET 2- WEST APPROACH
 FROM RI ROUTE 24**

SHEET DESIGNATION
CA-2



ROUTE 24 SOUTHBOUND SHOULDER CLOSURE

SCALE: 1"=40'

LEGEND

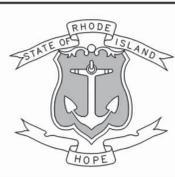
- RIDOT APPROVED TEMPORARY SIGN SUPPORT
- ADVANCE WARNING ARROW PANEL
- FLUORESCENT TRAFFIC CONES R.I. STD. 26.1.0
- POLYETHYLENE DRUM BARRICADE WITH MARKINGS STD. 26.2.0
- PLASTIC PIPE BARRICADE STD. 26.3.0
- POLICE VEHICLE WITH FLASHING BAR
- PORTABLE CHANGEABLE MESSAGE SIGN
- DIRECTION OF TRAFFIC FLOW
- WORK SPACE

MAINTENANCE AND PROTECTION OF TRAFFIC NOTES:

1. ALL TRAFFIC CONTROL DEVICES AND TEMPORARY TRAFFIC CONTROL ZONE ACTIVITIES SHALL MEET THE REQUIREMENTS OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) 2009 EDITION AND ALL REVISIONS.
2. TEMPORARY GUIDE SIGNS, DIRECTIONAL SIGNS, AND ROUTE SIGNS USED IN TEMPORARY TRAFFIC CONTROL ZONES SHALL HAVE A BLACK LEGEND AND BORDER ON AN ORANGE BACKGROUND.
3. TEMPORARY CONSTRUCTION SIGNS AND BARRICADES SHALL BE IN PLACE PRIOR TO THE START OF WORK.
4. ALL SIGNS AND BARRICADES ARE TO BE PLACED AND RELOCATED AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
5. CONES SHALL ONLY BE USED WHEN A TRAFFIC CONTROL SET UP IS USED DURING WORKING HOURS AND IT IS SUBSEQUENTLY BROKEN DOWN AT THE END OF THE WORKDAY.
6. SIGN MOUNTINGS SHALL BE IN ACCORDANCE WITH THE R.I. SPECIFICATIONS FOR TEMPORARY CONSTRUCTION SIGNS.
7. TEMPORARY CONSTRUCTION SIGNS SHALL BE REMOVED OR COVERED WHEN WORK IS NOT TAKING PLACE.
8. POLICE DUTY OFFICERS SHALL BE POSITIONED AT ALL SIGNALIZED INTERSECTIONS AND AS DIRECTED BY THE ENGINEER.
9. POLICE DUTY OFFICERS AND FLAGPERSONS SHALL BE ACTIVELY ENGAGED IN DIRECTING TRAFFIC, PEDESTRIAN PROCEEDING AND COORDINATION OF CONSTRUCTION EQUIPMENT ACCESS AT ALL TIMES.

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**RHODE ISLAND
DEPARTMENT OF TRANSPORTATION**

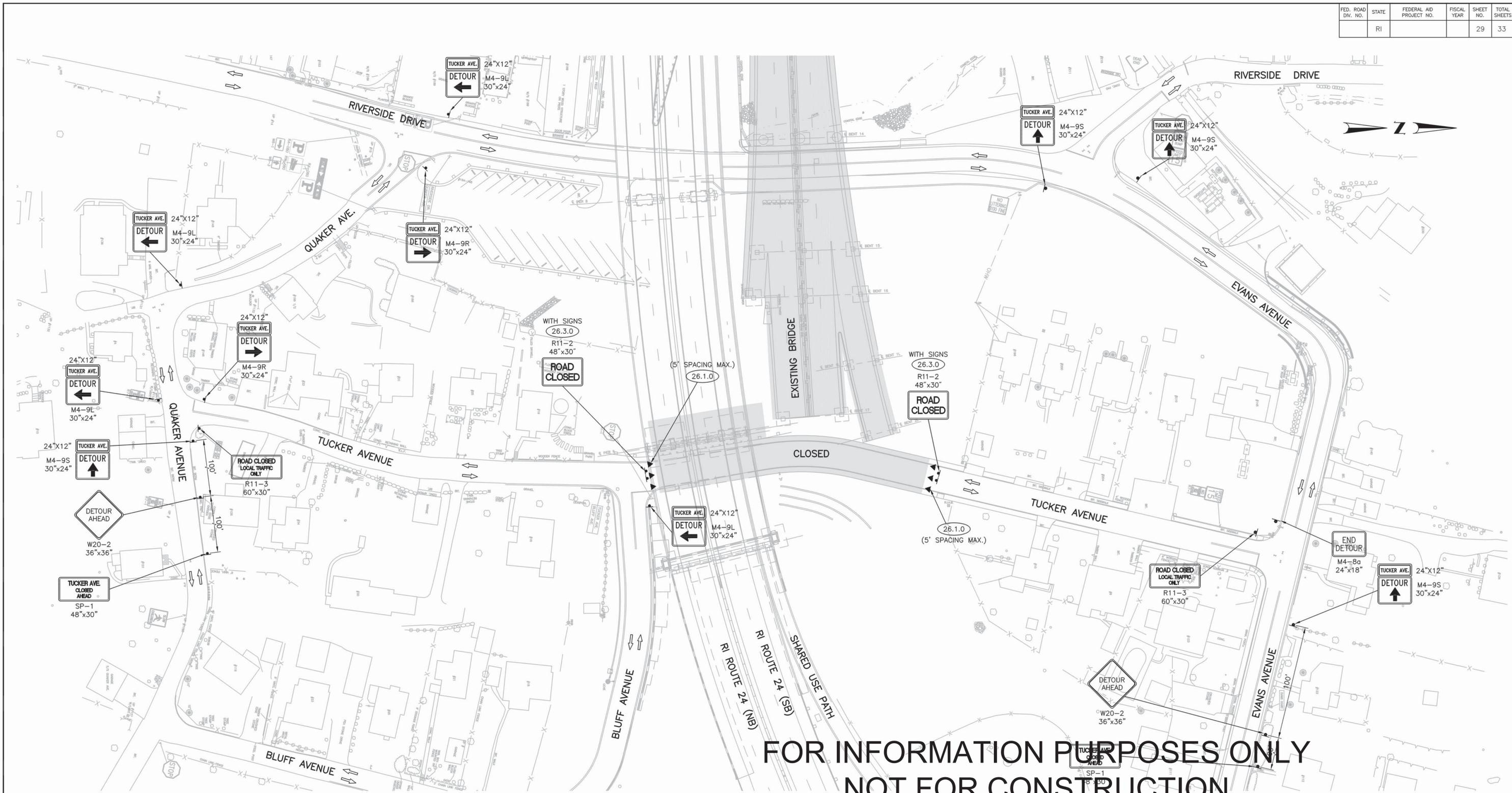
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SCALE: 1"=100'					

**BRIDGE DEMOLITION
OLD SAKONNET RIVER BRIDGE NO. 250**

PORTSMOUTH / TIVERTON RHODE ISLAND

**MAINTENANCE AND PROTECTION
OF TRAFFIC PLAN NO. 1**

SHEET DESIGNATION
MPT-1



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TUCKER AVENUE CLOSURE & DETOUR
SCALE: 1"=40'

LEGEND:
 WORK SPACE

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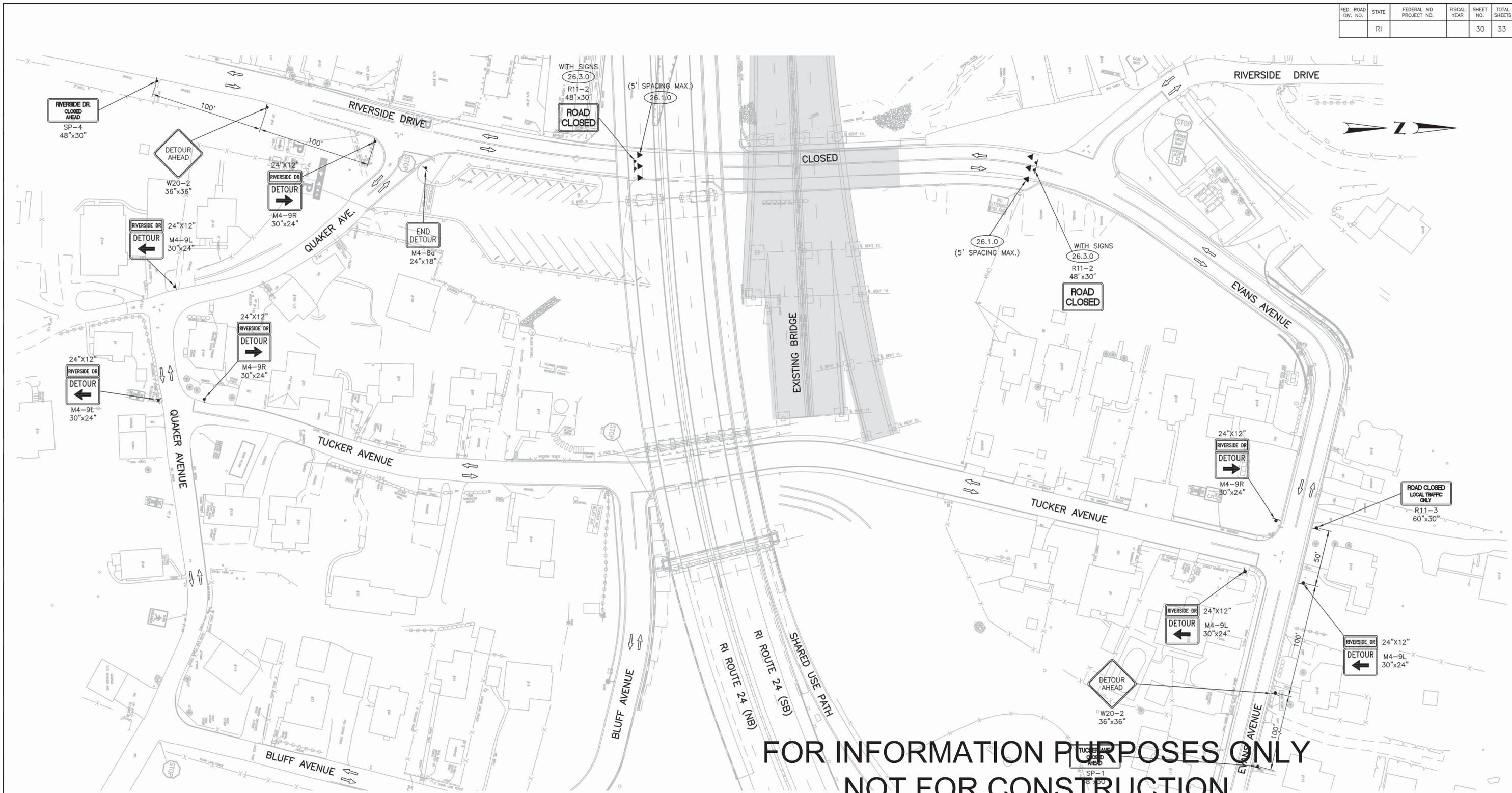
RHODE ISLAND
DEPARTMENT OF TRANSPORTATION

REVISIONS			REVISIONS		
NO.	DATE	BY	NO.	DATE	BY
DESIGNED BY:			DATE:		
CHECKED BY:			DATE:		
SCALE: 1"=40'					

BRIDGE DEMOLITION
OLD SAKONNET RIVER BRIDGE NO. 250
PORTSMOUTH / TIVERTON
RHODE ISLAND

MAINTENANCE AND PROTECTION
OF TRAFFIC PLAN NO. 2

SHEET DESIGNATION
MPT-2



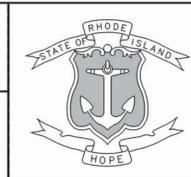
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RIVERSIDE DRIVE CLOSURE & DETOUR
SCALE: 1"=40'

LEGEND:
 WORK SPACE

CEC# 03062.00

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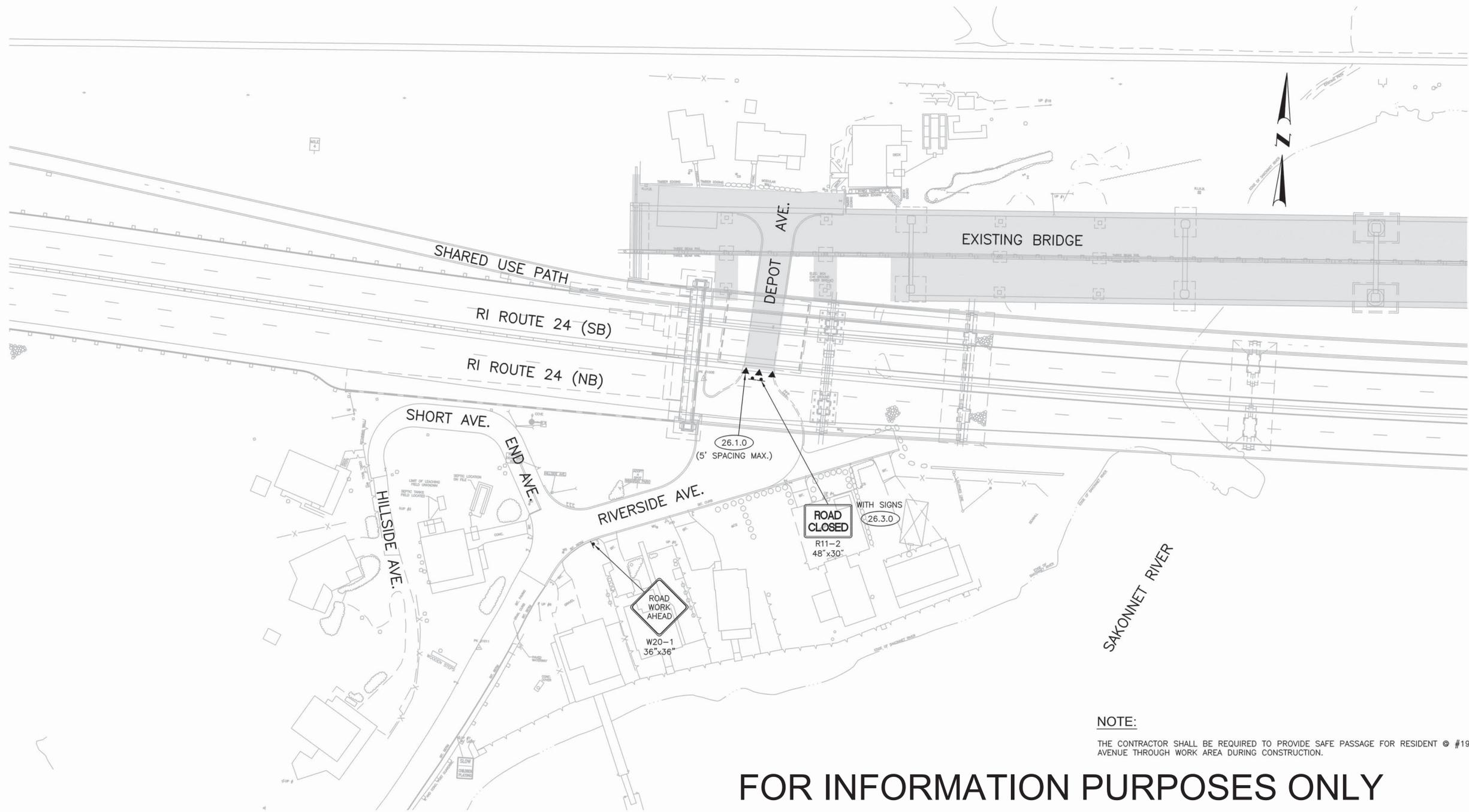
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CHECKED BY:			DATE:		
SCALE: 1"=40'					

BRIDGE DEMOLITION
OLD SAKONNET RIVER BRIDGE NO. 250
PORTSMOUTH / TIVERTON
RHODE ISLAND

MAINTENANCE AND PROTECTION
OF TRAFFIC PLAN NO. 3

SHEET DESIGNATION
MPT-3



NOTE:
 THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE SAFE PASSAGE FOR RESIDENT @ #190, #196, AND @203 DEPOT AVENUE THROUGH WORK AREA DURING CONSTRUCTION.

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DEPOT AVENUE CLOSURE
 SCALE: 1"=40'

LEGEND:
 WORK SPACE

CEC# 03062.00

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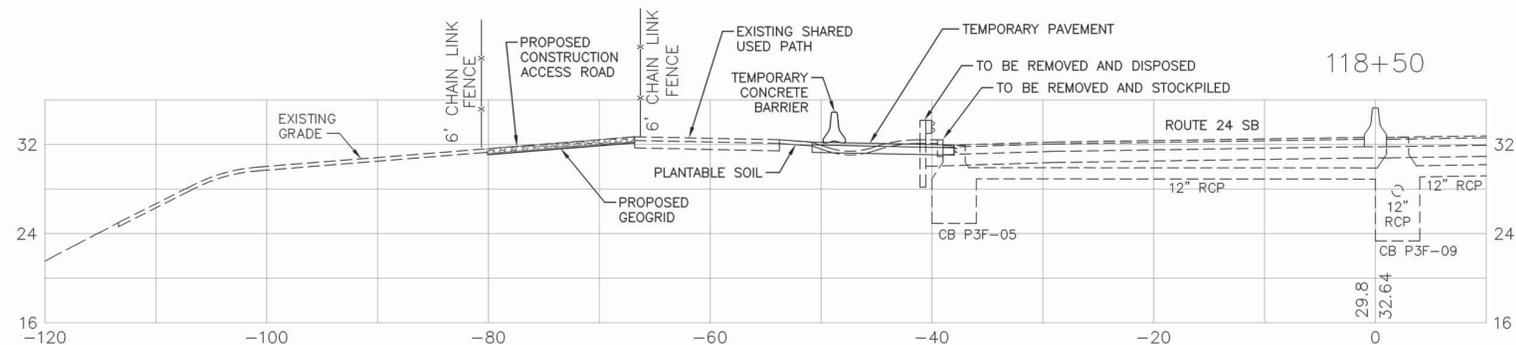
REVISIONS			REVISIONS		
NO.	DATE	BY	NO.	DATE	BY
DESIGNED BY:			DATE:		
CHECKED BY:			DATE:		
SCALE: 1"=40'					

BRIDGE DEMOLITION
OLD SAKONNET RIVER BRIDGE NO. 250
 PORTSMOUTH / TIVERTON
 RHODE ISLAND

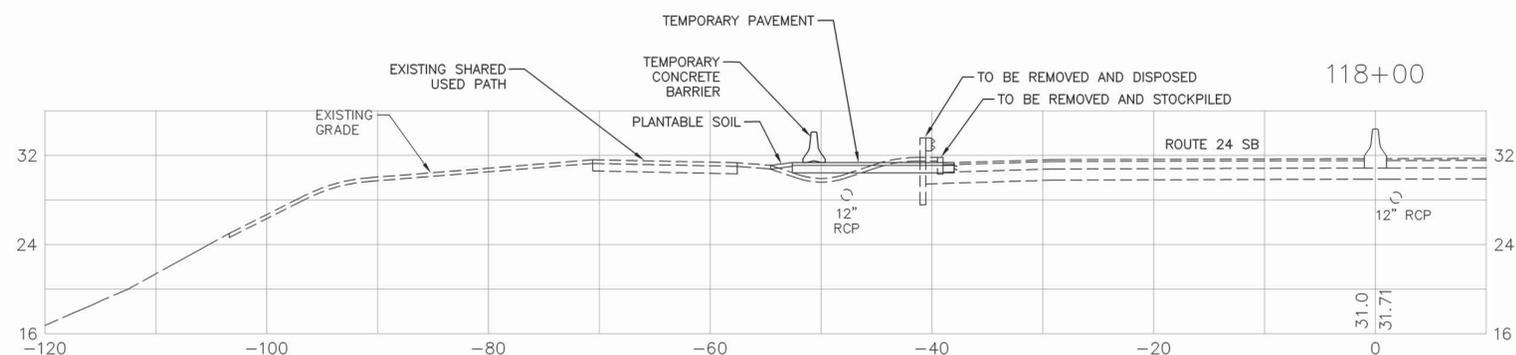
MAINTENANCE AND PROTECTION
 OF TRAFFIC PLAN NO. 4

SHEET DESIGNATION
MPT-4

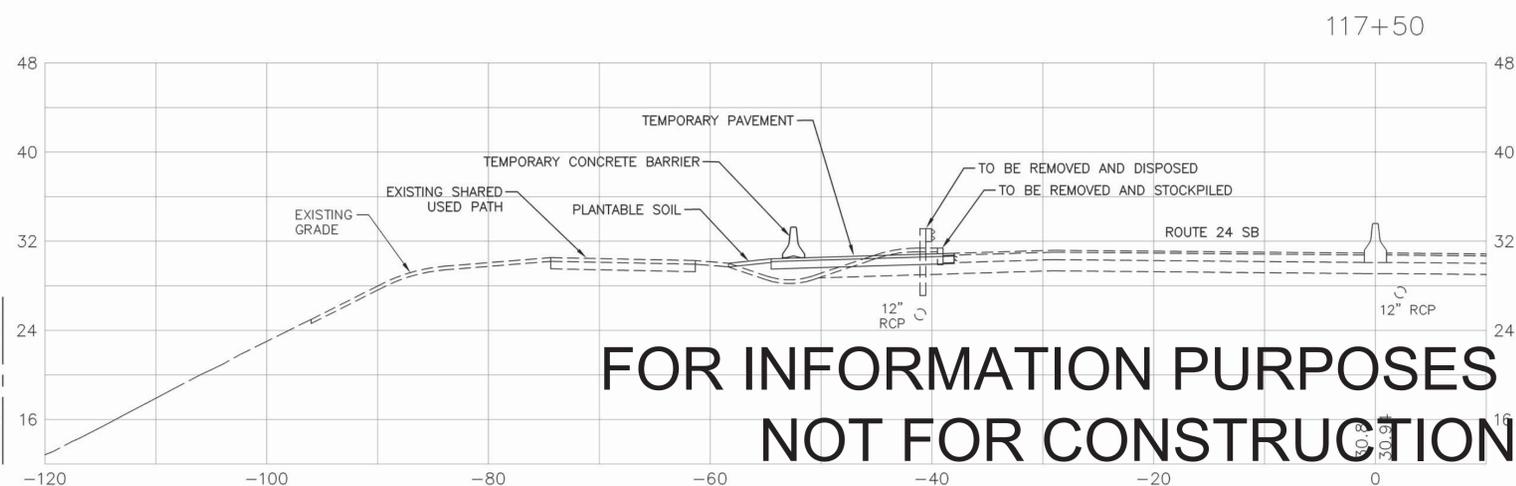
S.F.L. PLAT NO. 932



S.F.L. PLAT NO. 932



S.F.L. PLAT NO. 932



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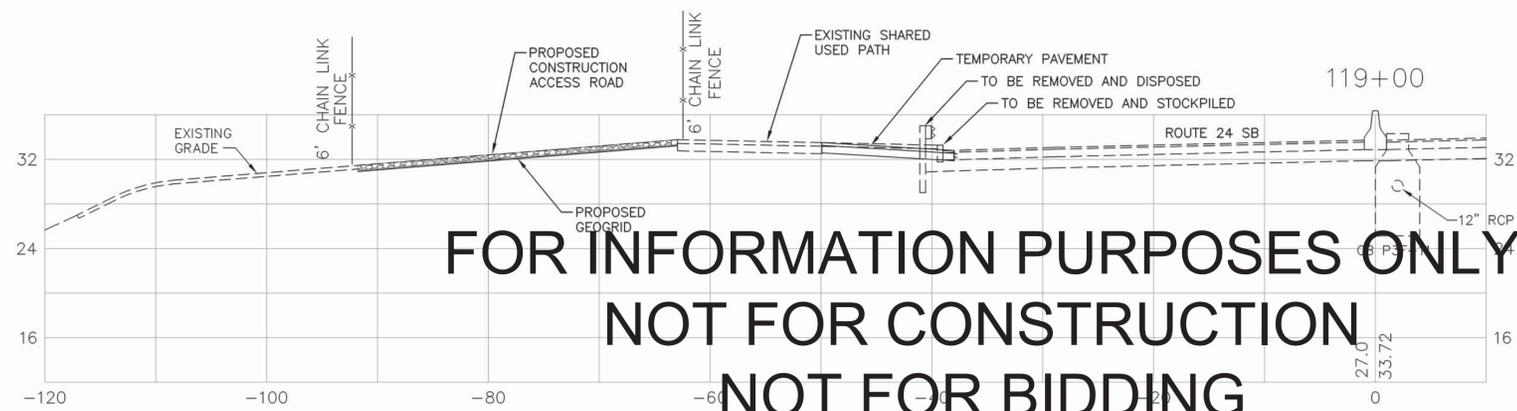
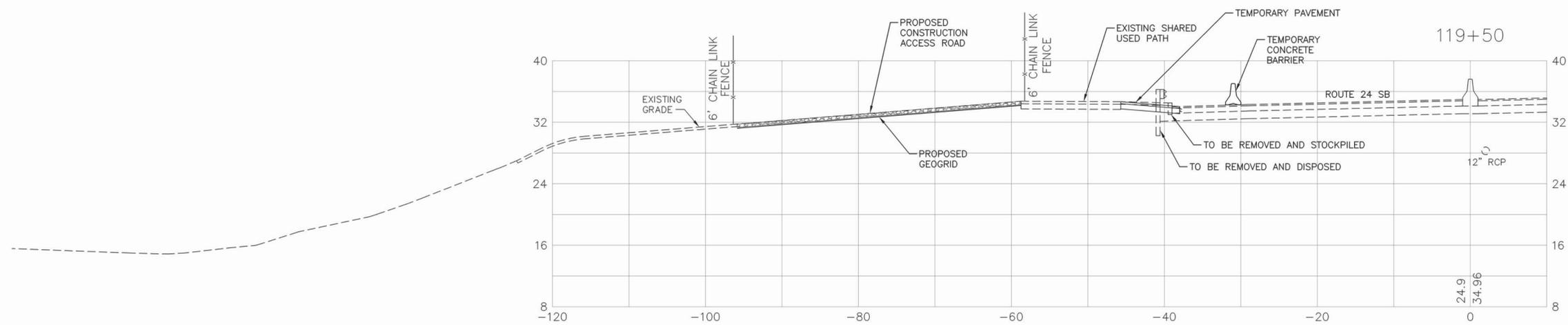
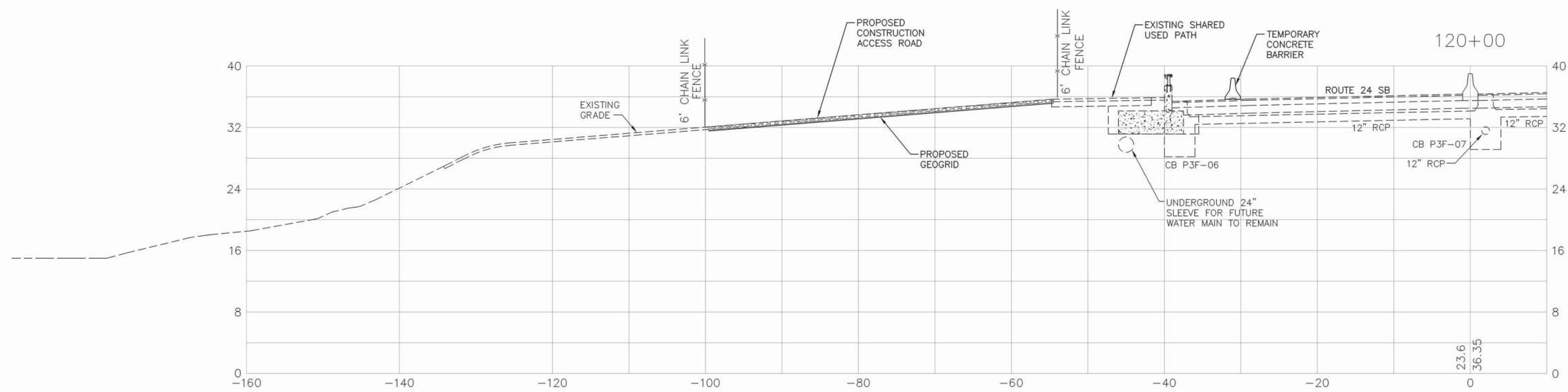
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SCALE: 1"=8'					

**BRIDGE DEMOLITION
OLD SAKONNET RIVER BRIDGE NO. 250**
PORTSMOUTH / TIVERTON
RHODE ISLAND

**ROUTE 24
CROSS SECTION NO. 1**

SHEET DESIGNATION



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SCALE: 1"=8'					

**BRIDGE DEMOLITION
OLD SAKONNET RIVER BRIDGE NO. 250**

PORTSMOUTH / TIVERTON RHODE ISLAND

**ROUTE 24
CROSS SECTION NO. 2**

SHEET
DESIGNATION